

WOKING DESIGN SPD

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CONTENTS

PART A - THE CONTEXT	5	8 WEST BYFLEET AND OTHER SMALLER SUBURBAN CENTRES	58
1 INTRODUCTION	1	8.1 Context	58
1.1 Background	1	8.2 West Byfleet	58
1.2 How to use this guidance	2	8A. Medium to high density mixed-use development	60
1.3 Approach	5	9 BOROUGH SUBURBS AND VILLAGES	64
1.4 Historic overview	5	9.1 Context	64
2 POLICY AND GUIDANCE CONTEXT	7	9A. Medium to high density (30-70 dph)	66
2.1 National policy	7	9B. Low to medium density (15-45 dph)	71
2.2 Sub-regional level guidance	9	9C. Lowest density/Arcadian (2-15dph)	76
2.3 Local policy	11	9D. Residential extensions	79
3 WOKING EVOLUTION AND URBAN CHARACTER REVIEW	19	10 CLIMATE CHANGE	85
3.1 Woking Character Study	19	10.1 Background	85
		10.2 Principles	85
PART B - TALL BUILDINGS	23	11 SHOPFRONTS	88
4 WOKING TALL BUILDINGS STRATEGY	25	11.1 Context	88
4.1 Introduction	25	11.2 Principles	88
4.2 Building heights in woking	26	PART E - GOOD DESIGN & THE PLANNING PROCESS	93
4.3 Tall buildings constraints	27	12 PLANNING APPLICATION REQUIREMENTS	95
4.4 Tall buildings Strategy	28	12.1 Introduction	95
PART C - KEY PRINCIPLES FOR CREATING SUCCESSFUL PLACES	31	12.2 Considerations relating to key themes	96
5 URBAN DESIGN PRINCIPLES	33	12.3 Supporting information	97
5.1 Introduction	33	APPENDICES	103
5.2 Design Principles	34	A1 WOKING TOWN CENTRE ANALYSIS	105
PART D - BOROUGH WIDE GUIDANCE	39	A2 WEST BYFLEET ANALYSIS	117
6 URBAN DESIGN GUIDANCE	41	A3 ADDITIONAL DESIGN RELATED DOCUMENTS	123
6.1 Introduction and conditions	41		
7 WOKING TOWN CENTRE	44		
7.1 Context Summary	44		
7A. High density mixed use development	46		
7B. Tall Buildings	53		

PART A - THE CONTEXT



Aerial view of Woking town centre (Woking Borough Council, 2007)

1 INTRODUCTION

1.1 BACKGROUND

Purpose of the Woking Design SPD

The purpose of the Woking Design Supplementary Planning Document (SPD) is to provide design guidance and good practice to help developers, applicants and Council officers improve the quality of design in new development across the borough.

In supplementing key policies contained in the adopted Woking Core Strategy, the Design SPD will be used by:

- planning officers to assess the design quality of development proposals when determining planning applications and offering pre-application advice;
- council members when assessing development proposals in advance of and at planning committee; and
- applicants and developers when preparing their schemes.

The Woking Character Study, 2010, identified a need for borough-wide design guidance to help ensure that new development is delivered to an appropriately high standard of design. Given the range of environments across the borough, from the dense urban centre to leafy suburbs, the guidance is necessarily wide-ranging. This SPD provides design guidance to assist in the management of development proposals across this range of environments, from large commercial developments through to small scale domestic modifications.



Jubilee Square development opened in 2012



West Byfleet district centre identified for growth



Recent high density development in Woking town centre



Future town centre redevelopment is planned around the bandstand

1.2 HOW TO USE THIS GUIDANCE

This guidance has been prepared to help improve design quality in new development across the Borough of Woking. It is a material planning consideration and will be used by Development Management Officers when assessing planning applications. The guidance is organised and structured as follows:

PART A provides an overview of the Borough-wide policy and environmental context within which this SPD has been prepared and includes:

- **2. Policy context** – which provides an overview of the national, regional and local planning policy context. This section also provides a review of the existing local guidance prepared by the Council. Much of this existing guidance remains relevant and should be read in conjunction with this Design SPD.
- **3. Woking evolution and urban character review** – which provides an overview of the Woking Character Study of 2010. This is an important background document for this SPD and the character types and areas identified in the guidance have directly informed the structure of the design guidance provided here.

PART B provides a tall buildings strategy for Woking.

- **4. Woking tall buildings strategy** – which assesses, tests and sets out a criteria for the suitability of tall buildings in Woking town centre.

PART C outlines key urban design principles for creating successful places.

- **5. Key urban design principles** – based on established best practice, borough-wide urban design principles which are relevant, either in whole or part, to all new development in the borough.

PART D provides the borough-wide design guidance, the structure of which is based on the Woking Character Study.

- **6. Borough wide guidance** – an introduction
- **7. Woking town centre** – which provides guidance relating to high density mixed-use developments in Woking town centre together with guidance on proposals for tall buildings.
- **8. West Byfleet and other smaller suburban centres** – which provides design guidance for West Byfleet and relevant to other smaller suburban commercial centres.
- **9. Borough suburbs and villages** – having categorised the Borough into different urban conditions based on the Woking Character Study, this section provides design guidance on the design of new housing developments across the borough.
- **10. Climate change** – prepared in tandem with the Council's climate change SPD, guidance here relates to how the design of new development should take account of issues associated with climate change.
- **11. Shopfronts** – design guidance relating to new shopfronts.

PART E relates to the information that should be prepared to support planning applications.

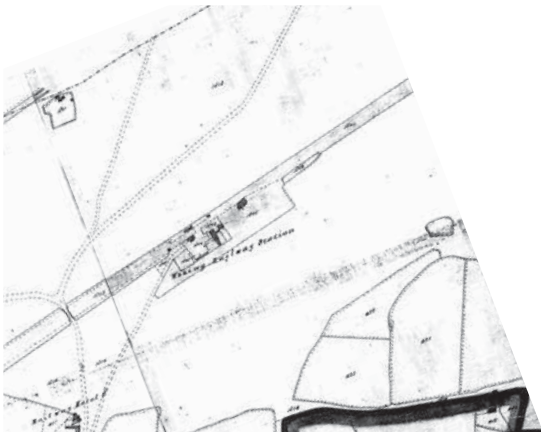
- **12. Planning application requirements** – provides advice on the illustrative material which should be submitted with planning applications to ensure proposals are able to demonstrate they have taken full account of context.

APPENDICIES which outline the urban design analysis underpinning the design guidance for Woking and West Byfleet centres.

- **A1. Analysis of Woking town centre** – environmental analysis of the centre which underpins the tall buildings strategy.

- **A2. Analysis of West Byfleet district centre** – environmental analysis of the centre undertaken to inform new development.
- **A4. Additional design related documents**
 - other documents produced by Woking Borough Council providing useful design related guidance that should be referred to in addition to the Woking Design SPD.

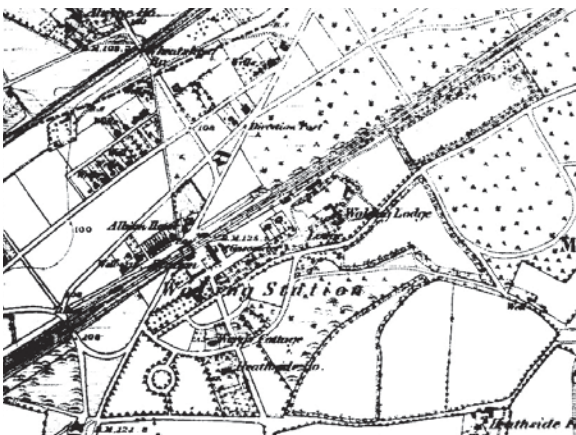
The evolution of Woking Town Centre



Tithe map - 1841



1885-1915



1872



1945-1970



The Surrey countryside dominates the character outside of urban areas



The rail service from Woking station has been instrumental in supporting growth

1.3 APPROACH

Context

Woking's role and prominence as a commercial and retail centre has grown significantly in recent decades. The Woking Core Strategy outlines an ambitious programme for continual growth and investment. The Woking Design SPD will play an important role in delivering high standards of design in new development.

Woking Borough Council has developed a strong vision on energy and climate change issues. This SPD seeks to match the Borough's credentials in the sustainability of new development with high standards of design.

The Design SPD is relevant to the borough as a whole. However, the SPD has necessarily given weight and emphasis to the areas likely to accommodate greatest change such as Woking town centre.

1.4 HISTORIC OVERVIEW

Although Woking can trace the origins of its settlement back to the Domesday Book of 1086 and was the location for one of Henry VIII's royal palaces, the town today is the product of rapid Victorian growth and subsequent twentieth century redevelopment and expansion. The cutting of the canal and construction of the railway across heathland, close to what is now referred to as Old Woking, opened up a new urban area. This has resulted in a series of phases of development and urban expansion accompanied by successive waves of suburban growth.

The town centre has continued to change and adapt through the twentieth century and from the late 1970s onwards, the Peacocks Centre, Peacock Arts and Entertainment Centre and Civic Centre, as well as the widening of the A320 Victoria Way, have all influenced Woking's current urban structure.

Beyond the central area, 60% of the borough is designated as green belt with generous parks, open spaces, golf courses and heathlands that offer good access to green space. The Basingstoke Canal and the River Wey are the two principal waterways running through the borough, both of which have significant design implications.

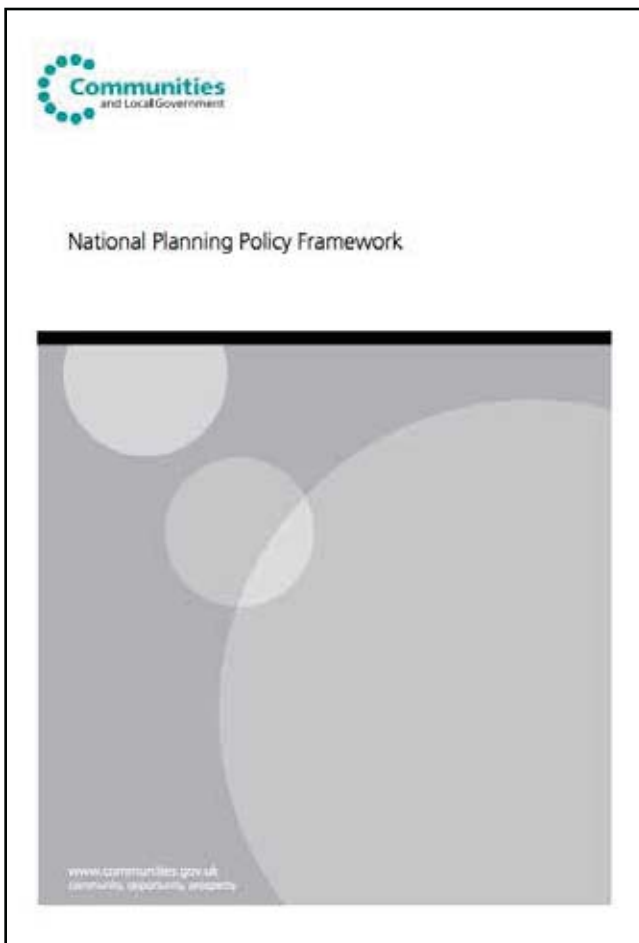
Woking benefits from excellent transport links with good access to the regional and national road network. London is 25 minutes away by train with frequent services to and from London Waterloo and is less than an hour away from both of London's principal airports. Guildford is 10 minutes away by train. This makes Woking an excellent location for both commuters and businesses that wish to be located away from the centre of London but also require access to good connections.



**EVANS
CYCLES**

2 POLICY AND GUIDANCE CONTEXT

2.1 NATIONAL POLICY



National Planning Policy Framework

Introduction

This section summarises the policy context for the Design SPD. Key policy documents and guidance from national, sub-regional and local level have been reviewed.

National Planning Policy Framework

The National Planning Policy Framework (NPPF) is an important part of the government's reforms to simplify the planning system. The NPPF has replaced the series of Planning Policy Guidance and Statements adopted over the last decade.

The NPPF states that the purpose of the planning system is to contribute to the achievement of sustainable development. It also introduces a presumption in favour of sustainable development, emphasising that this should be the 'golden thread' that runs throughout plan making and decision-making on planning applications.

The NPPF emphasises the importance of good design:

"The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people." (Paragraph 56)

"Local and neighbourhood plans should develop robust and comprehensive policies that set out the quality of development that will be expected for the area. Such policies should be based on stated objectives for the future of the area and an understanding and evaluation of its defining characteristics." (Paragraph 58)

"Permission should be refused for development of poor design that fails to take the opportunities

available for improving the character and quality of an area and the way it functions.” (Paragraph 64)

Permitted development rights for change of use

The Government updated the General Permitted Development Order on 30 May 2013. The following changes have been adopted:

- Agricultural buildings under 500 sqm can change to a number of other uses (A1, A2, A3, B1, B8, C1 and D2). For buildings between 150 sqm and 500 sqm, prior approval (covering flooding, highways and transport impacts, and noise) is required.
- Up until the 30 May 2016, premises in B1(a) office use can change to C3 residential use, subject to prior approval covering flooding, highways and transport issues and contamination.
- Premises in B1, C1, C2, C2A and D2 use classes can change use permanently to a state-funded school, subject to prior approval covering highways, transport impacts and noise and contamination.
- Buildings with A1, A2, A3, A4, A5, B1, D1 and D2 uses will be permitted to change use for a single period of up to two years to A1, A2, A3 and B1 uses.
- Thresholds for business change of use have been increased from 235 sqm to 500 sqm for permitted development for change of use from B1 or B2 to B8 and from B2 or B8 to B1.

The Government further updated Permitted development rights in 2014, with the following changes coming into force from the 6 April 2014:

- New class IA allows change of use and some associated physical works from a small shop or provider of professional/financial service (A1 and A2) to residential use (C3), subject to prior

approval. Up to 150 sqm of retail space will be able to change to residential use. This new right does not apply to land protected by article 1(5) of the General Permitted Development Order.

- New class CA allows change of use from a shop (A1) to a bank or a building society.
- New class MB allows change of use and some associated physical works from buildings used for agricultural purposes to residential use (C3), subject to prior approval. Up to 450 sqm of retail space will be able to change to up to three dwellings. This new right does not apply to land protected by article 1(5) of the General Permitted Development Order.
- Change of use from offices (B1), hotel (C1), residential (C2 and C2A), non-residential institutions (D1), and leisure and assembly (D2) to nurseries providing childcare, subject to prior approval.
- New class MA allows change of use from buildings used for agricultural purposes to a state funded school or nursery providing childcare, subject to prior approval.

Extending permitted development rights for homeowners

The changes to the General Permitted Development Order in May 2013 also included changes to the permitted development of residential extensions. The main change is to increase the size limits for the depth of single-storey domestic extensions from 4m to 8m (for detached houses) and from 3m to 6m (for all other houses), in non-protected areas, for a period of three years.

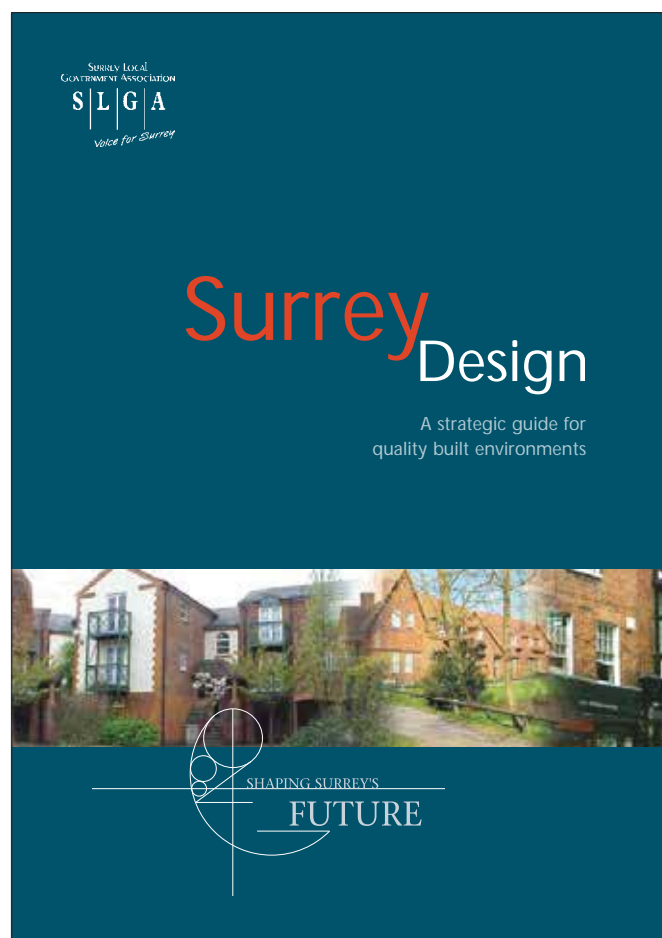
2.2 SUB-REGIONAL LEVEL GUIDANCE

Surrey Design Guide (2002)

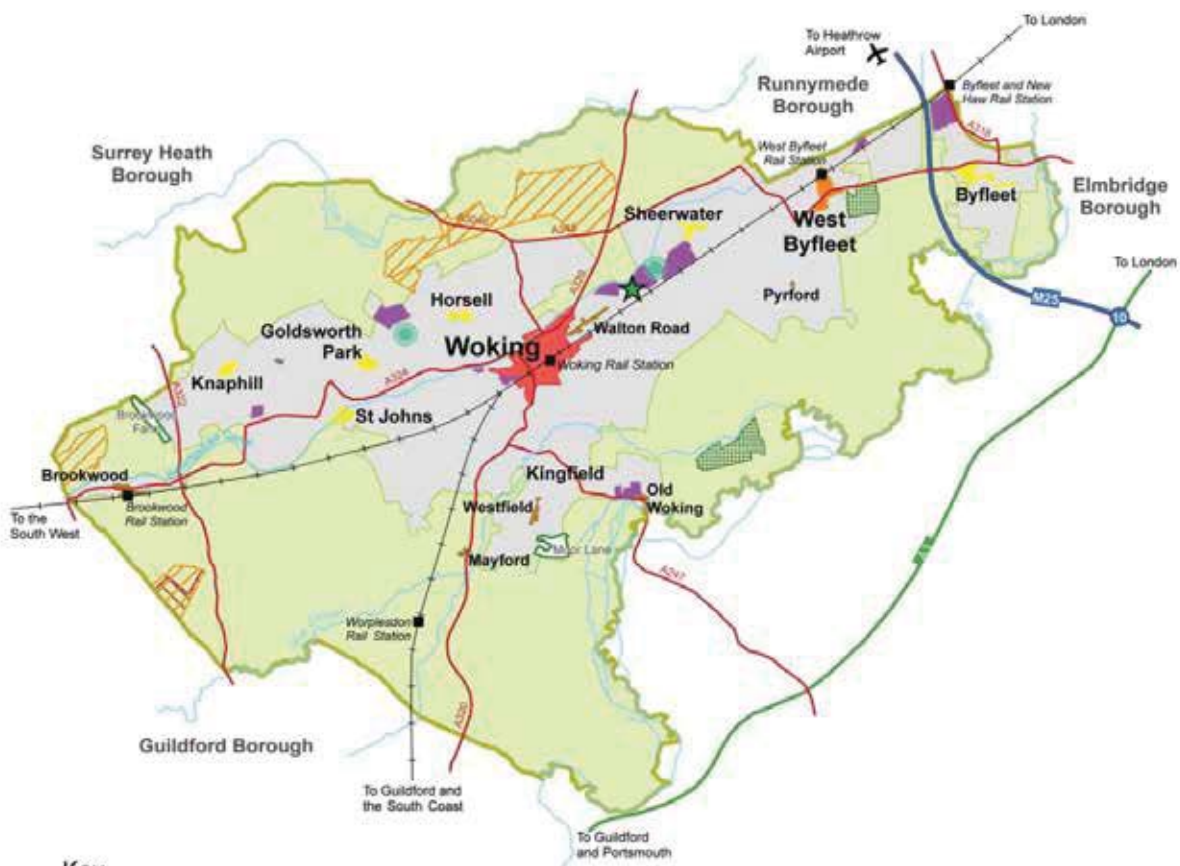
The Surrey Design Guide is a county level SPD and is therefore a material consideration. It defines good design as “an holistic process that brings together social, environmental and economic needs with aesthetic concerns.” The guide is structured around the following objectives:

- Process: To promote good design through the development process.
- Quality: To create attractive and accessible places with reference to scale, height, form, mass, proportion and rhythm, colour, and texture.
- Character: To ensure that all development contributes to local distinctiveness and character.
- Resources: To conserve energy and water, maintain biodiversity and reduce waste and pollution.
- People: To encourage vibrant and mixed communities where people feel safe.
- Movement: To create places for people that are safe and easy to move through and are accessible to all.
- Land: To make best use of the available land.

Under the heading of quality, the guide provides a useful set of issues to be considered in building design. This guidance is directly relevant to Woking (see Chapter 2, pg 18).



Surrey Design Guide



Key

- Borough Boundary
- Urban Area
- Town Centre
- District Centre
- Local Centres
- Neighbourhood Centres
- Employment Areas
- Safeguarded Sites
- Priority Places
- New access road
- Green Belt
- Major Developed Sites in the Green Belt
- Thames Basin Heath Special Protection Areas
- Special Area of Conservation
- Railway

Not all land designations are shown, please view in conjunction with the Woking Borough Proposals Map.



Woking Core Strategy Key Diagram

2.3 LOCAL POLICY

Woking Core Strategy (2012)

The Woking Core Strategy was adopted in October 2012 and provides the vision for the Borough until 2027. This vision aims to enhance the existing strengths and opportunities, while addressing Woking's weaknesses and threats. The spatial vision for the Borough includes the following statements which emphasise the importance of design quality:

"Woking will be a Borough of high environmental quality and standards where people choose to live, work and visit, an exemplar in the achievement of sustainable growth." (Paragraph 3.2)

"New development in the Borough will be well designed to respect the character of the area. New housing development will help to meet the needs of all sections of the community." (Paragraph 3.2)

The vision is supported by 13 objectives (Paragraph 3.) which include the following:

"To enable a diverse range of development such as offices, housing, shops, leisure and cultural facilities in Woking Town Centre to enable its status as a centre of regional significance to be maintained. Development will be of high quality and high density to create an attractive environment for people to live, do business and visit."

"To enable attractive and sustainable development of the district and local centres to provide convenient access to everyday shops and local services, local community facilities, parks and open spaces."

"To enable the provision of well designed homes of different types, tenures and affordability to meet the needs of all sections of the community. This will be in sustainable locations and at densities that maximise the efficient use of urban land without compromising the distinctive character of the local area."

"To lead the way in high quality sustainable development that minimises the adverse impacts of climate change. This will be achieved through maximising opportunities for implementing renewable energy technologies, maximising the efficient use of energy and water in buildings and managing waste effectively."

"To encourage the high quality design of buildings, neighbourhoods and the public realm that creates a sense of place where people feel safe to spend time and interact without fear of crime."

There are a series of adopted planning policies in the Core Strategy, which the SPD will directly support. These include the following:

CS21: Design

This policy sets out the Council's aim that all forms of development should make a positive contribution to the environment and strengthen the character and distinct identity of the area. The policy highlights the following aspects of design, which need to be considered when planning for new development.

- scale;
- height;
- proportions;
- building lines;
- layout;
- materials;
- relationship to adjoining properties;
- accessibility considerations;
- landscape setting;
- boundary treatments; and
- future adaptability.

The policy states that tall buildings can be developed in Woking town centre, if they are well designed and justified in the context.



Old Woking



West Byfleet



West Byfleet



Horsell Common



Knaphill



The River Wey

The supporting text to Policy CS21: Design states "Woking Borough Council expects the design of buildings to be inclusive and comprehensive, not only looking at aesthetic quality, but also how the development brings various elements together to improve the quality of life of its users and enhance the character of the area."

CS20: Heritage and Conservation

There are 25 conservation areas in the Borough and 180 listed buildings which help to contribute to Woking's diverse built character. Policy CS20: Heritage and Conservation requires new development to respect and enhance the character and appearance of an area, and make a positive contribution to the character, distinctiveness and significance of the historic environment.

The explanatory text to the policy emphasises that new development should take account of the character within which it is situated, highlighting both the Woking Character Study and the Heritage of Woking report as useful resources to applicants. The policy also requires applications to include detailed plans relating to materials and the relationship to adjoining properties.

CS10: Housing provision and distribution

The Council will make provision for 4,964 net additional dwellings in the Borough between 2010 and 2027. Policy CS10 establishes indicative density ranges for new development across the borough relating to the location or type of development. The policy states "Wherever possible, density should exceed 40 dwellings per hectare and will not be justified at less than 30 dwellings per hectare, unless there are significant constraints on the site or where higher densities cannot be integrated in to the existing urban form."

It should be noted that within this Design SPD densities below 30dph are discussed. These densities are only relevant to small scale development in the least urban locations, where there is a need to respect existing density and character.

CS22: Sustainable Construction

Policy CS 22: Sustainable Construction requires all new development to maximise efficient use of energy and water, adapt to the impacts of climate change, facilitate the reduction of waste and meet the BREEAM and Code for Sustainable Homes (or any future national equivalent). All residential development is expected to meet energy and water components of Code Level 4. The policy requires new residential development on Greenfield sites to meet the full requirements of Code Level 5. New non-residential development should comply with BREEAM 'very good' standards.

Policy CS22: Sustainable Construction includes the following statements specifically relating to design and construction:

"The design of all new developments will be required to take account of layout, landform, orientation and landscaping to maximise efficient use of energy and adapt to the impacts of climate change." (CS22 pg 107)

"The design of all new developments should facilitate the reduction of waste and the recycling and composting of the waste produced." (CS22 pg 107)

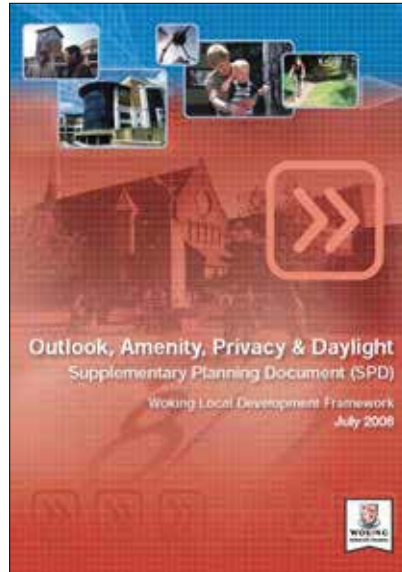
"All developments should consider the use of sustainable construction techniques that promote the reuse and recycling of building materials. All development is encouraged to use responsible resourcing of materials and is encouraged to source materials locally. All new residential development is encouraged to meet the 'materials' elements of the Code." (CS22 pg 108)

CS24: Woking's Landscape and Townscape

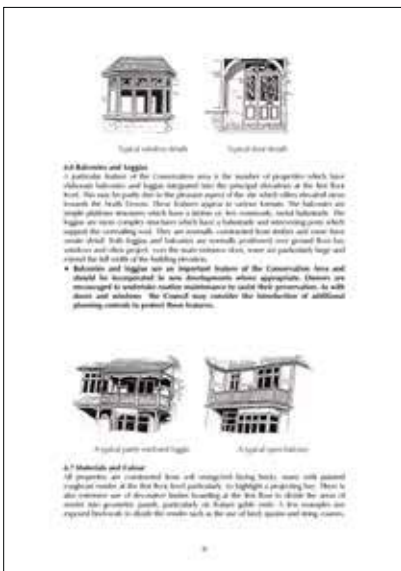
In order to protect the local landscape and townscape character, Policy CS24: Woking's Landscape and Townscape outlines that new development will be expected to conserve, and where possible enhance, existing landscape and townscape character, respect the setting of, and relationship between settlements and individual buildings, support land management practices that have no adverse impact on biodiversity and landscape patterns and encourage the planting of new trees.



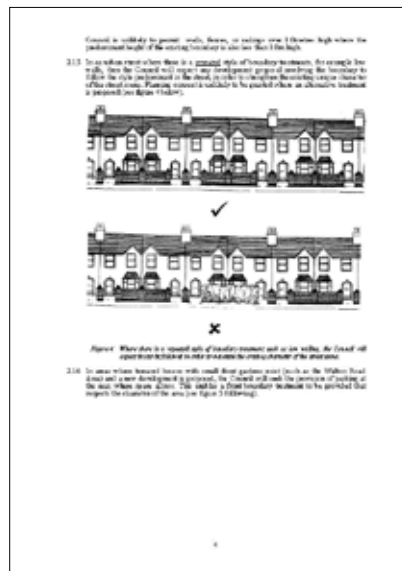
Woking Character Study



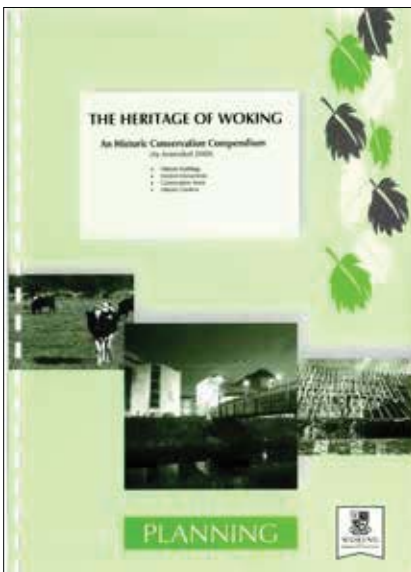
Outlook, Amenity, Privacy & Daylight SPD



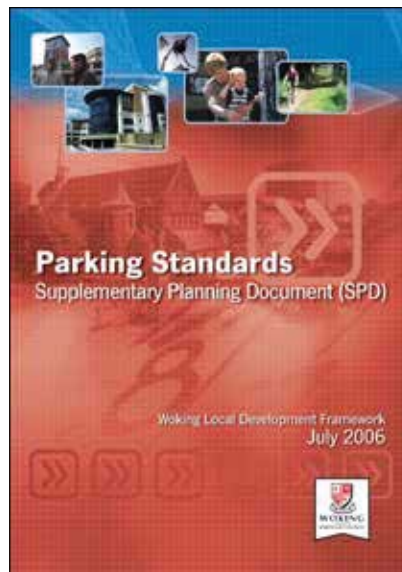
Conservation Area Statements



Residential Boundary Treatments



Heritage of Woking report



Parking Standards SPD

CS5: Priority Places

The Design SPD should help to address the Borough's Priority Places including the ward of Maybury and Sheerwater and the Lakeview Estate area of Goldsworth Park by emphasising the importance of enhancing open spaces, amenity areas, and by proposing developments which overlook public areas thereby contributing to a safe environment. As part of the programme for the Priority Places, new development in the Maybury and Sheerwater ward will include 250 homes and additional employment and retail space.

CS2: Woking Town Centre

Policy CS2 supports Woking town centre as the primary centre for economic development and as the preferred location for high density residential development. The policy requires new development proposals to deliver high quality, well designed public spaces and buildings, which make efficient use of land, contribute to the functionality of the centre and add to its attractiveness and competitiveness.

CS3: West Byfleet District Centre

Policy CS3: West Byfleet District Centre encourages high density mixed use development within the district centre, emphasising that all new development should be well designed and integrated, and enhance local character.

Parking Standards SPD (2006)

Parking standards were developed in support of Policies MV9 and MV12 of the Woking Borough Local Plan (1999) which relate to Off Street Parking and Cycle Parking Standards. These standards aim to control the supply of parking in new residential and workplace developments in order to encourage sustainable travel.

The Parking SPD is underpinned by the following objectives:

A: To control the supply of parking in new developments in order to support travel without using a car in line with regional and national guidance.

B: To encourage a reduction in workplace parking.

C: To apply restraint-based parking standards (i.e. maximum standards) in order to achieve an average of no more than 1.5 spaces per new dwelling across the borough.

D: To reduce levels of car parking in new developments within the High Accessibility Zone to reflect the higher level of accessibility by choice of means of transport.

E: To recognise the local context and assist with the implementation of the emerging Local Development Framework Core Strategy.

Parking Standards for Woking are currently under review.

Outlook, Amenity, Privacy and Daylight SPD (2008)

This document originally supplemented Policy HSG21 of Woking Borough Local Plan (1999) on achieving suitable outlook, amenity, privacy and daylight in new residential developments whilst safeguarding those attributes of adjoining residential areas. The guidance is still considered relevant.

The SPD highlights the following considerations under each of the themes:

- **Outlook** – ensuring that the close proximity of another building (or other controlled works) does not adversely affect accommodation by diminishing the visual enjoyment of a dwellings' immediate setting.
- **Amenity** – the provision of landscaped space or other outdoor amenity surrounding dwellings usually forming; private amenity space (private realm), and the landscaped frontage to the development (public realm).
- **Privacy** – the protection of habitable rooms and intimate areas of private outdoor amenity from being directly overlooked.
- **Daylight** – the amount of natural daylight required to illuminate internal rooms. Reference is also made to providing access to sunlight, and the safeguarding of access to sunlight for solar energy generation.

Climate Change SPD (2013)

This document has been produced to provide detailed information regarding the implementation and delivery of Core Strategy Policy CS22: Sustainable Construction and CS23: Renewable and low carbon energy generation. The SPD gives guidance on achieving sustainable construction standards and incorporating renewable and low carbon energy technologies, which will have an impact on the design of development. It also sets out design considerations to enable development to better mitigate and adapt to climate change, emphasising the important of green infrastructure.

Residential Boundary Treatments SPG (2000)

This guidance was originally supplementary to Policy BE1 in the Woking Borough Local Plan (1999) and outlines guidance on boundary treatments for new housing developments for both individual properties and developers. Of key importance is the type of boundary that separates the private space of individual properties with that of the public realm of the street, balancing the issues of security while maintaining appropriate character.

The guidance in the SPG states that "*the type of boundary treatment chosen should always reinforce the existing character of the area and follow what is the predominant boundary type in that locality*".

The guidance sets out specific advice for four types of residential area: the Green Belt, Urban Areas of Special Residential Character (UASRC), Urban and Town Centre locations, and Open Plan developments.

House Extensions SPG (2011)

This guidance was originally supplementary to Policy HSG23 of the Woking Borough Local Plan 1999 and sets out guidance on the suitable design of domestic extensions to dwellings. It does not cover extensions to commercial and other properties.

Although many extensions can be undertaken without planning permission under 'Permitted

Development Rights' the guidance states that "Most types of construction will need to satisfy Building Regulations, for which consent must be obtained" and that "Properties which are Statutory Listed or within a designated Conservation Area are subject to additional planning controls, which may require a separate approval."

Conservation Area Statements

There are 25 conservation areas within the Borough of Woking. The Statements describe the special qualities of each conservation area.

Statements have been prepared for the following Conservation Areas and will be reviewed and updated in due course:

- Mount Hermon
- Old Woking
- Ashwood Road
- Horsell
- Byfleet Corner, Rosemount Parade and Station Approach
- Pond Road
- St Johns
- Wheatsheaf

Further appraisals for the other conservation areas are being prepared.

The Heritage of Woking report (2000)

This document provides information and details of the historic buildings, ancient monuments and historic gardens across Woking and its conservation areas and the measures and policies then in place to protect them.

The appendices to the report include additional guidance on the retention of buildings on the local list and Council grants available for funding, as well as a very useful summary description and map of each of the Conservation Areas designated.

Town, District and Local Centres Study (2009)

This document forms part of the retail and leisure evidence base for the Local Development Framework and reviews the vitality and viability of the main town centre of Woking and the Borough's other district local centres including West Byfleet and Knaphill.

The study assessed the need and deficiencies in floorspace for both retail and leisure. Table 3.1 of the study draws together the comparison and convenience retail capacity, as well as the leisure orientated food and drink capacity across the whole of Woking Borough. This assessment is based on constant market shares and expenditure but should be carefully monitored and kept under review.

Affordable Housing Delivery SPD (2014)

This document has been produced to provide detailed information regarding the implementation and delivery of Core Strategy Policy CS12: Affordable housing. It explains what applicants and developers need to do to meet the requirements of the policy and is an important document to help deliver the vision and objectives of the Core Strategy.

Hot Food Takeaway SPD (2014)

The SPD provides detailed guidance to ensure that future Hot Food Takeaway development is of the highest design standards, does not create significant harm to neighbouring properties and does not lead to an over proliferation of Hot Food Takeaway outlets in the borough's retail centres and parades.

Emerging local guidance

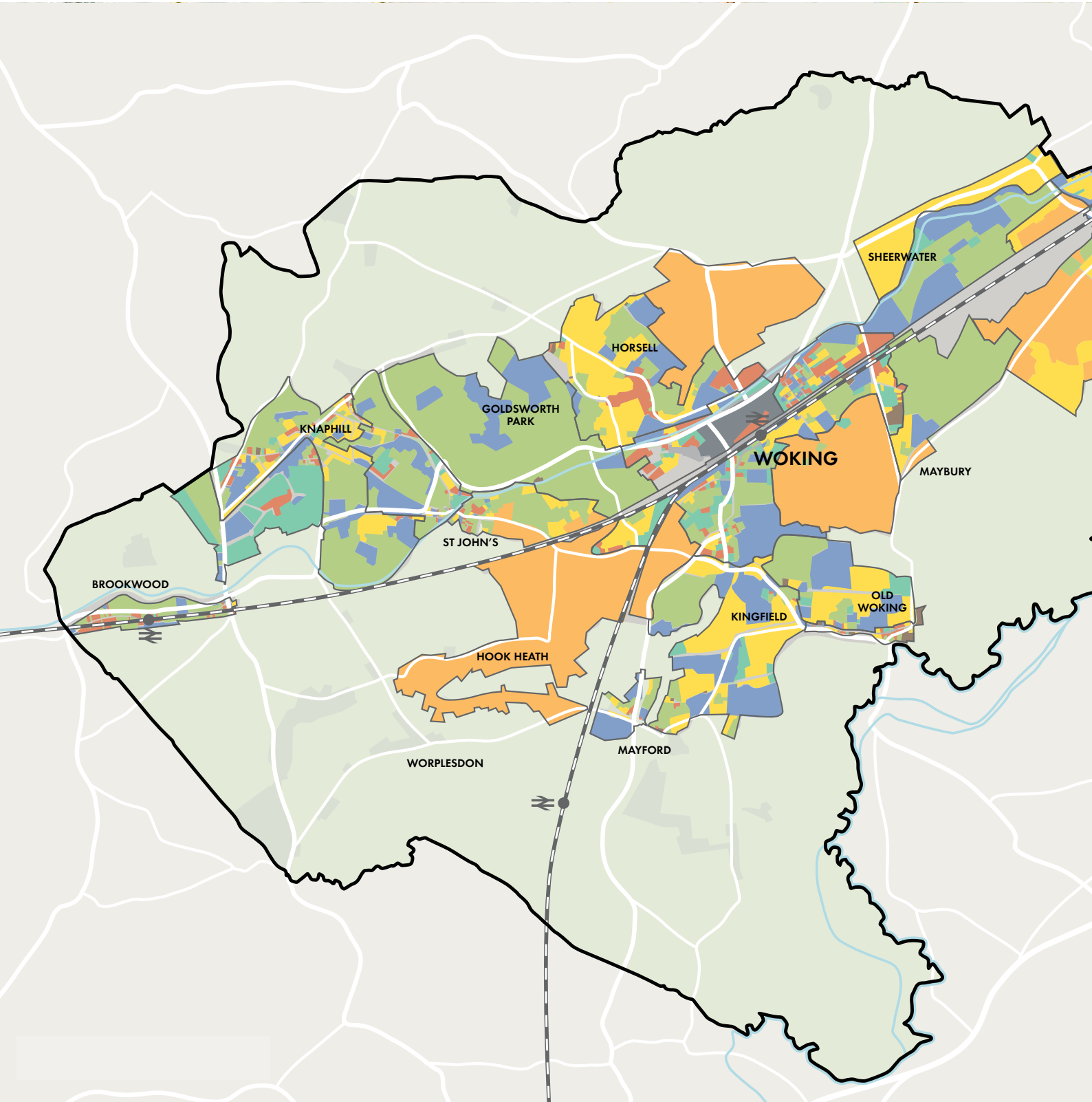
The Council is currently working on the following documents which will emerge in due course:

- Woking Delivery DPD - which will combine the site allocations and development management policies.
- Green Infrastructure Strategy
- Parking Standards SPD

Each of the above will influence the location and nature of development and its design, and therefore reviews of these emerging documents and have informed the Design SPD.



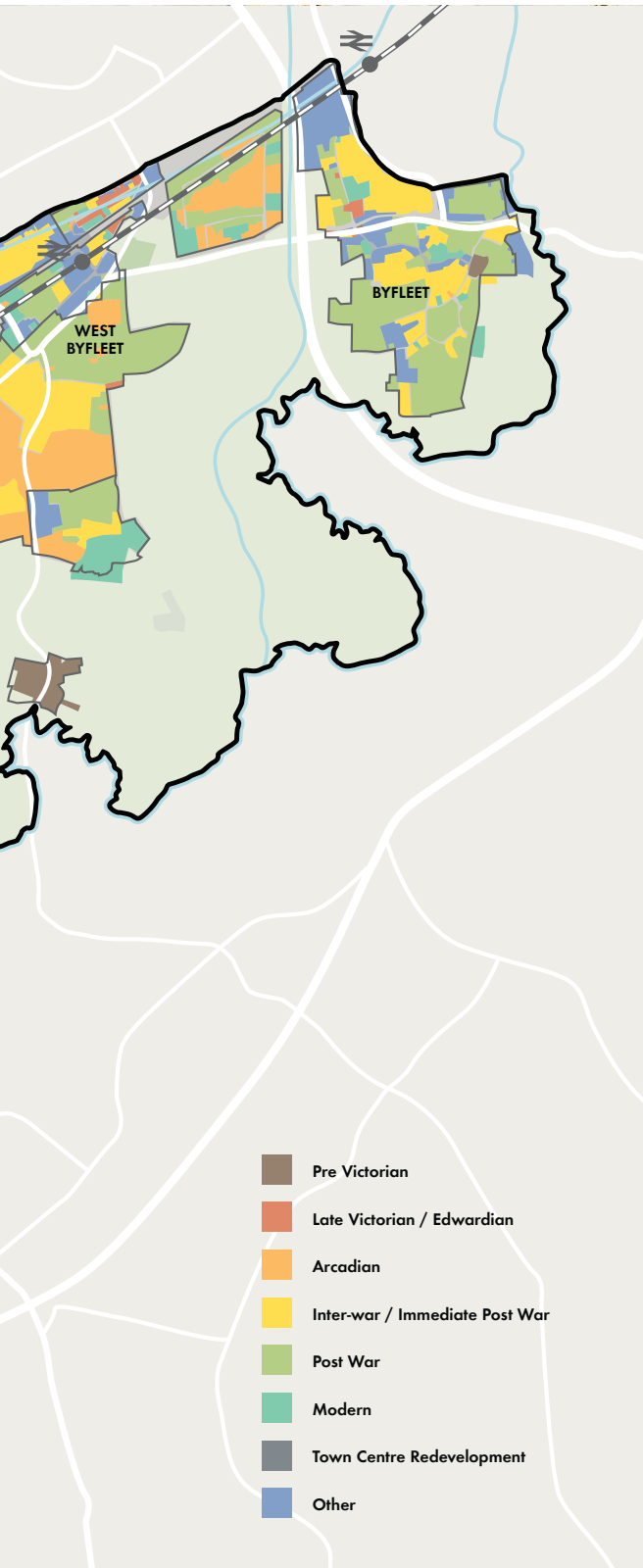
Example of guidance from the Residential Boundary Treatments SPG - "Demolition of boundaries to provide parking erodes the character of the street scene"



Woking Character Study building typologies across the borough (as identified by 2010 study by The Landscape Partnership, mapped by Allies and Morrison Urban Practitioners)

3 WOKING EVOLUTION AND URBAN CHARACTER REVIEW

3.1 WOKING CHARACTER STUDY



This study considers the built up areas of Woking Borough in terms of 30 character areas. Key features of the Borough are identified such as historic development, infrastructure, green belt, biodiversity and heritage. This is an important study and a key part of the evidence base for the Design SPD.

The character study divides the borough's urban areas into the following typologies:

- Pre-Victorian
- Late Victorian/Edwardian
- Arcadian
- Inter-war/immediate post war
- Post war
- Modern
- Town centre redevelopment
- Other

These have been mapped for the purpose of the Woking Design SPD, the plan shows the predominance of Arcadian and Post war estates in the suburban areas of Woking and West Byfleet.

The character study drew some important conclusions that helped to inform the Woking Core Strategy. These include the following considerations for the Core Strategy Design and Heritage policies:

- *Within Woking Borough, the areas currently designated as Conservation Areas are generally those with the strongest character and should be protected. Vegetation, including trees and hedges, is often a key element in the character of these areas and should be retained as far as possible.*
- *Areas within the Inter-war/immediate post war and post war typologies often have the largest areas of open space, which should be protected, but the developments are not always of the highest quality*



Pre-Victorian



Late Victorian / Edwardian



Arcadian



Inter-war / Immediate Post-war



Post-war



Modern

in design terms and are often not locally distinctive. Future development within these areas should seek to rectify this situation where possible.

- Woking town centre has undergone significant redevelopment since the 1960s, which has led to the loss of many historic buildings. It is important to conserve and enhance older buildings that have been retained and to ensure that new developments are designed and developed to the highest possible quality.*
- The character areas on the periphery of Woking town centre are some of the most variable in the Borough, with Victorian buildings often located adjacent to modern, often non-residential, buildings. This variety can enhance the character of the areas but can also undermine local character if developments of different types do not complement each other.*

PART B - TALL BUILDINGS



WILLIAM E. CLAYTON
 GEORGE C. CORE
 FRANK C. CORBETT
 JAMES C. CORBETT
 ROY F. A. COCKY
 CIL B. COFFIN
 ARTHUR ALD COLE
 WILLIAM T. COLE
 RENEY COLLIER
 FRED W. COLLIER
 THOMAS COLLIER
 W. A. COLLYER
 BERT G. COLLYER
 BERT F. G. COOMBER



4 WOKING TALL BUILDINGS STRATEGY

4.1 INTRODUCTION

Woking has recently seen the development of a number of tall buildings and it is important that a framework is established for the assessment of any further proposals. As such, this section sets out the strategy for tall buildings in Woking town centre. It will be important that development proposals coming forward in Woking Town Centre take proper account of this strategy and seek to respond positively to the opportunities and constraints identified.

The strategy begins by mapping existing building heights in Woking Town Centre, then examining the constraints to tall buildings and concludes with a strategy to guide future tall building development.

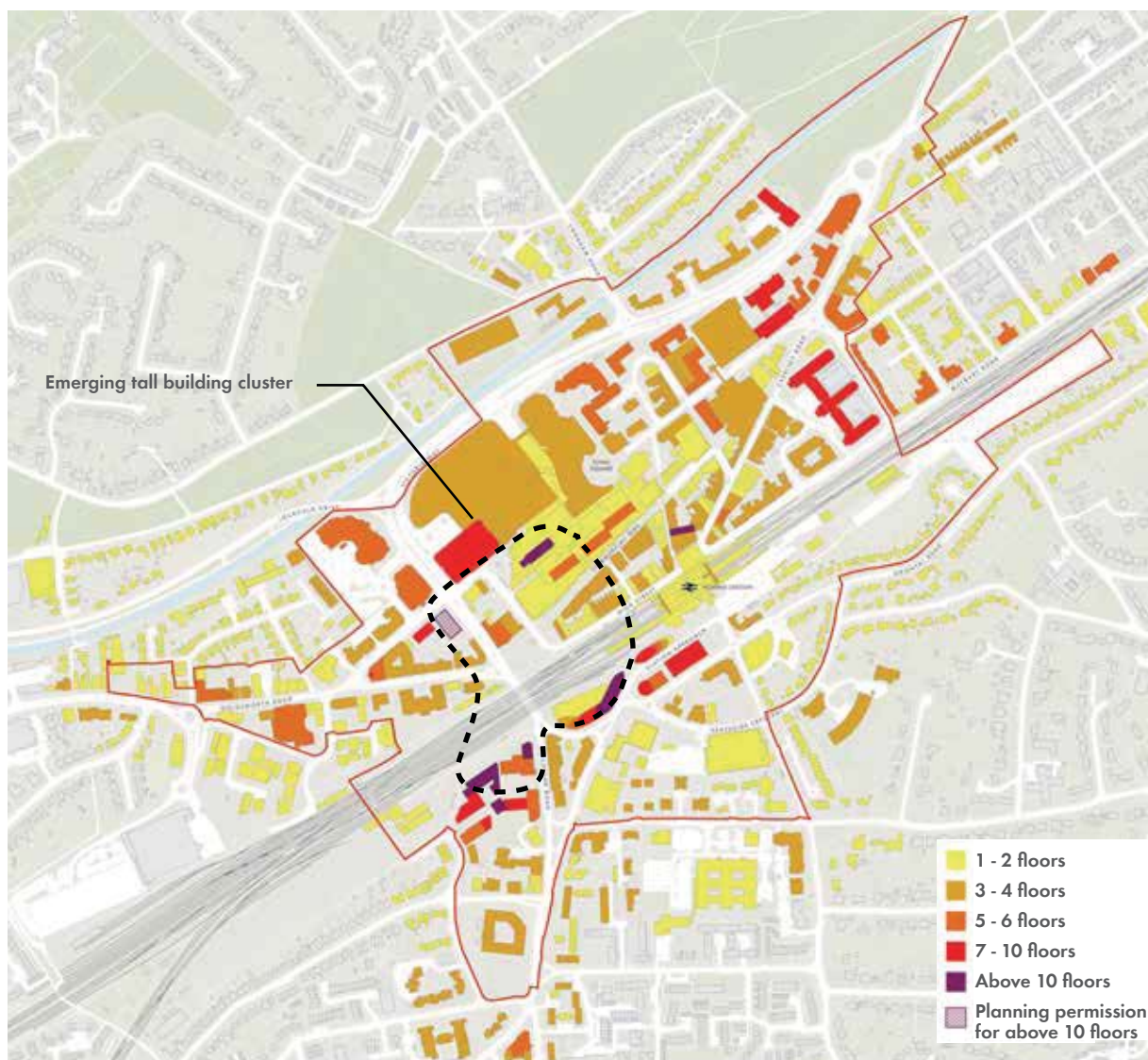
4.2 BUILDING HEIGHTS IN WOKING

The plan below shows building heights across Woking Town Centre. It demonstrates that there is considerable variation in heights within the town centre but that there is a cluster of tall buildings emerging on the southern part of Victoria Way and to the south of the railway line.

Prevailing building heights are typically around 3-5 storeys. Given that tall buildings are defined as buildings which are significantly taller than those around them, buildings above 6 storeys in height would therefore be regarded as tall buildings in Woking town centre.

However, any building two or more storeys higher than neighbouring properties could be considered tall, regardless of whether it is over 6 storeys. Furthermore, if a proposal is located in an area where tall buildings are prevalent, it could still be considered tall even if it is not significantly taller than those around it.

To ensure clarity, all tall building applications should quote building heights, number of storeys and Ordnance Datum Level when presenting proposals and clearly demonstrate the proposal's relationship to adjacent building heights.



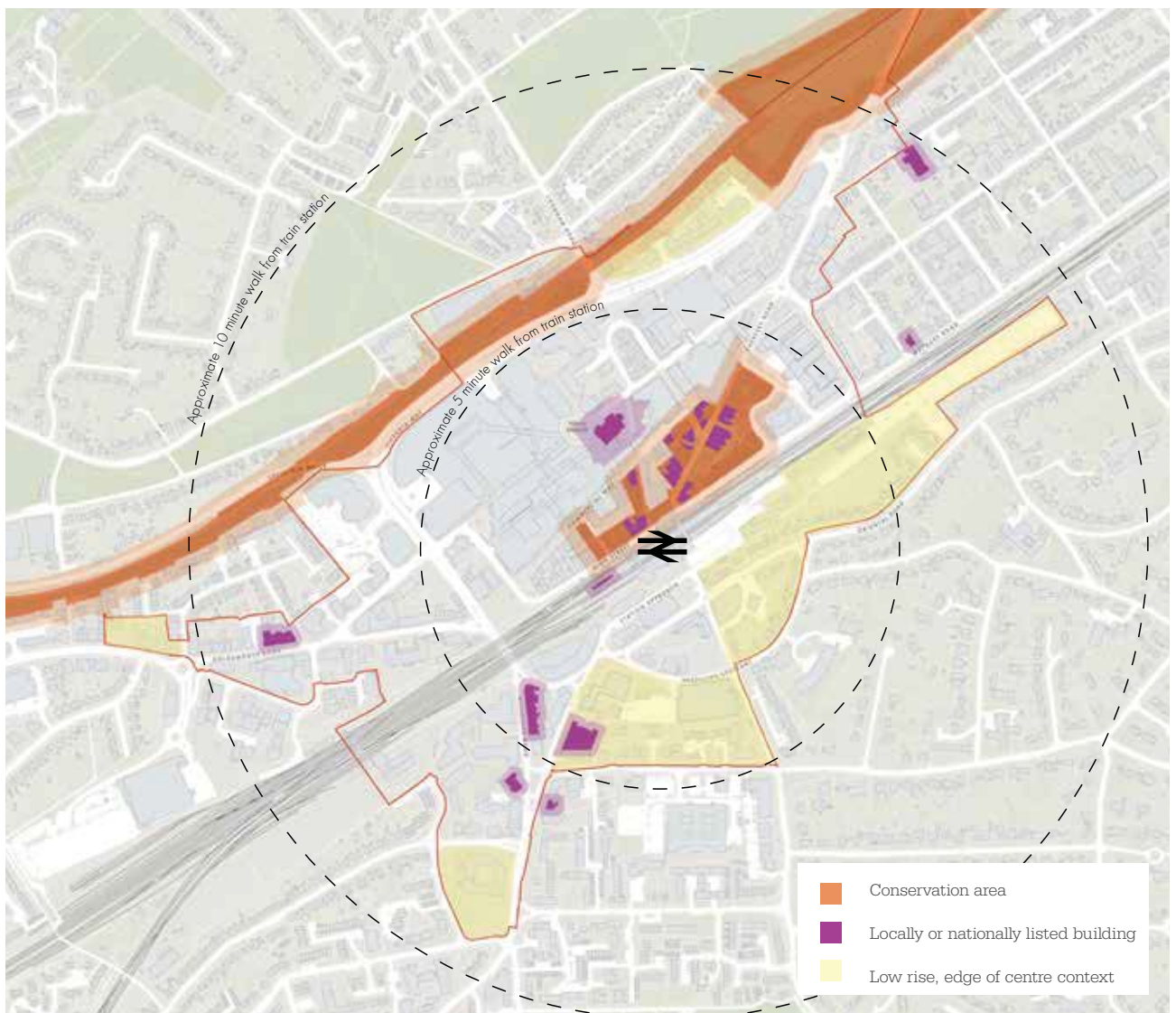
Existing heights within Woking town centre with the emerging tall building cluster highlighted

4.3 TALL BUILDINGS CONSTRAINTS

The setting of heritage assets, including conservation areas and listed buildings, is an important consideration when assessing an area's appropriateness for tall buildings. Areas with a predominantly low-rise character, which are outside the core of the town centre, are not considered suitable for tall buildings, regardless of a lack of recognised heritage assets.

The walk time from Woking Station is also an important consideration, with the excellent train services, combined with proximity to shops and services, creating a sustainable location for higher density development.

Woking train station is a major focal point for the town centre and the suitability of sites for tall buildings should be seen to decrease moving away from it.



Existing constraints impacting the suitability of tall buildings within Woking town centre

4.4 TALL BUILDINGS STRATEGY

Woking's strategy for tall buildings is based upon a series of tall building criteria against which proposals will be considered and assessed. More detailed design guidance relating to proposals for new tall buildings in Woking town centre is included in the SPD at Section 7B.

Informed by the guidance included in this SPD, proposals for tall buildings must be accompanied by analysis of the town centre in terms of public transport accessibility, proximity to community infrastructure, such as schools and GP services, and townscape qualities, which will influence the appropriateness of tall buildings in any given location. The criteria against which proposals for tall buildings will be considered include, but are not necessarily limited to, the following:

1. Be of exceptional design quality and subject to a formalised design review process during the evolution of the scheme;
2. Not adversely affect the site's surrounds in terms of micro-climate, wind, overshadowing, glare, aviation navigation and telecommunications interference;
3. Contribute positively to the setting of identified heritage assets that might be affected by the proposal;
4. Take account of key views both across the site and long views towards the building itself. Design proposals will need to take into account the need for the building to be designed so it is seen in the round; and
5. Pay particular attention to the environment created at ground floor. Proposals must be appropriate to the streets and spaces they address and should exploit opportunities for improvement of existing and creation of new public spaces.



Aerial view of Woking Town Centre showing Station area, Conservation Area and emerging tall building cluster area

**PART C - KEY
PRINCIPLES
FOR CREATING
SUCCESSFUL PLACES**



5 URBAN DESIGN PRINCIPLES

5.1 INTRODUCTION

This section outlines a set of overarching design principles, which should guide all proposed developments, regardless of scale or location.

The National Planning Policy Framework defines good design as development that is:

- attractive;
- durable; and
- usable.

A wealth of good practice guidance on design has been assimilated over the last 15 years, which has helped to underpin the understanding of good design in the UK. 'By Design - Urban Design in the Planning System' (published by the Department of the Environment, Transport and the Regions and the Commission for Architecture and the Built Environment in 2000) is one such document. 'By Design' identifies a core set of design principles which have informed the approach to urban design adopted by local authorities. Another particularly important piece of good practice guidance is "The Lifetime Homes Standard" which sets out criteria for building accessible and adaptable homes. The guidance includes car parking layout and access, approach and entrances to dwellings and internal arrangements that enable inclusive design. It should be noted that all public places and spaces should be designed to meet disability requirements where relevant. A third useful source of design information is Secured by Design, which is the UK Police flagship initiative supporting the principles of "designing out crime". "Designing Out Crime" addresses the concept of reducing anonymity of an offender and is associated with design issues such as the relationship between private and public space, environmental design and physical security measures.

The overarching design principles set out in this section draw heavily upon the set identified in By Design, whilst also emphasising aspects of particular importance to the borough of Woking.

5.2 DESIGN PRINCIPLES

CHARACTER

a place with its own identity



- Every development should be underpinned by a clear understanding of the local context and character, issues and opportunities.
- Each building and its external spaces should make a positive contribution to enriching the site, the street and the local area.
- The character of the development should be locally inspired and reference immediate typologies.

CONTINUITY & ENCLOSURE

a place where public and private spaces are clearly distinguished



- There should be a clear network of streets and building frontages.
- Streets should have strong building lines and, where appropriate, be lined with street trees.
- Buildings should be designed and positioned with landscaping to define and enhance streets and spaces.
- Regardless of height and scale, every building should engage with its street through activity provided by doors and windows that directly address the street.

QUALITY OF THE PUBLIC REALM

a place with attractive and successful outdoor areas



- Public spaces should be functional and actively attract their use.
- Public and private spaces should be clearly defined and designed to be attractive, functional, well managed and safe.
- Landscape is integral to the character of the borough's streets and must be appropriately recognised through proposals.

EASE OF MOVEMENT

a place that is easy to get to and move through



- New developments should be permeable, reinforcing existing and new connections, in order that they are well integrated with their surroundings.
- Parking should not undermine block structure or dominate the street.
- Access to public transport should be optimised.
- Streets should be designed to allow them to function as social spaces.

LEGIBILITY

a place that has a clear image and is easy to understand



- The development should be easy to find your way around, sitting comfortably within the existing context.
- Every home and business should have a street address.
- Streets should encourage activity and feel safe.

ADAPTABILITY

a place that can change easily



- The layout and form of spaces within a building should support changes in the way they are used over time.
- The perimeter block form represents the most adaptable and flexible urban form and this should be used wherever possible.

CLIMATE CHANGE

a place which faces up to climate challenges



- Developments should maximise opportunities for renewable energy, maximise the efficient use of energy and water and manage waste effectively.
- The design of buildings should incorporate passive measures from the outset to ensure optimum use of resources.
- Streets and spaces should be designed to be comfortable environments in extreme weather with adequate shelter and shade.

DIVERSITY

a place with variety and choice



- New developments should provide an appropriate mix of uses including community facilities.
- Residential components should seek to provide a range of housing types and tenures to meet local requirements.
- Developments should meet a variety of demands from the widest possible range of users.

INTEGRITY

a place which delivers its potential



- Developments should be informed by a clear set of objectives which are used to assess the scheme at each iteration.
- Proposals must invest appropriate attention to material choices and detailing.
- Each building should amply accommodate its intended occupants and include sizeable outdoor space and good internal/external storage.



COMMERCIAL HOUSE

PART D - BOROUGH WIDE GUIDANCE



6 URBAN DESIGN GUIDANCE

6.1 INTRODUCTION AND CONDITIONS

Introduction

This section outlines detailed design guidance for the borough. All design principles highlighted are expected to be taken into account when designing new schemes for Woking. The approach builds on the findings of the Woking Character Study. The plan overleaf links the character types identified in that study with a range of urban locations under which design guidance has been prepared in this Design SPD. These conditions are arranged as follows:

- Woking town centre - where significant high density development is planned.
- West Byfleet and other smaller suburban centres - where there are opportunities for medium-high density development and refurbishment to enhance these smaller centres.
- Borough suburbs and villages - encompassing the range of residential neighbourhoods across the borough.

Conditions

Each section begins with an example development site, which typifies the range of conditions needing to be addressed by proposals in these areas. Expected density levels for that area are applied and the proposals are tailored to respond to specific context and character.

7. Woking town centre

- A. High density mixed-use development
- B. Tall buildings

8. West Byfleet and other smaller suburban centres

- A. Medium-high density mixed use development

9. Borough suburbs and villages

- A. Medium to high density
- B. Low to medium density
- C. Lowest density (Arcadian)
- D. Residential extensions

Not all areas of the borough will neatly correspond to the Woking conditions identified. In these cases, it is likely that guidance from two or more conditions may be relevant. The borough's Priority Places are one such example where guidance in Section 9A & B will be relevant.

In addition, specific sections have been included at the end of this section providing guidance on shopfront design and climate change.

Guidance structure

The guidance has been structured to allow users to ensure designs are appropriately context-led. Each section starts with an overview of the character and context of the area, highlighting the qualities and issues that are unique to the area and need to be carefully considered from the outset.

Under each of the conditions identified in an area, guidance on the following aspects of design is provided:

- Layout
- Scale
- Streetscape
- Amenity
- Façades
- Climate change
- Access / parking

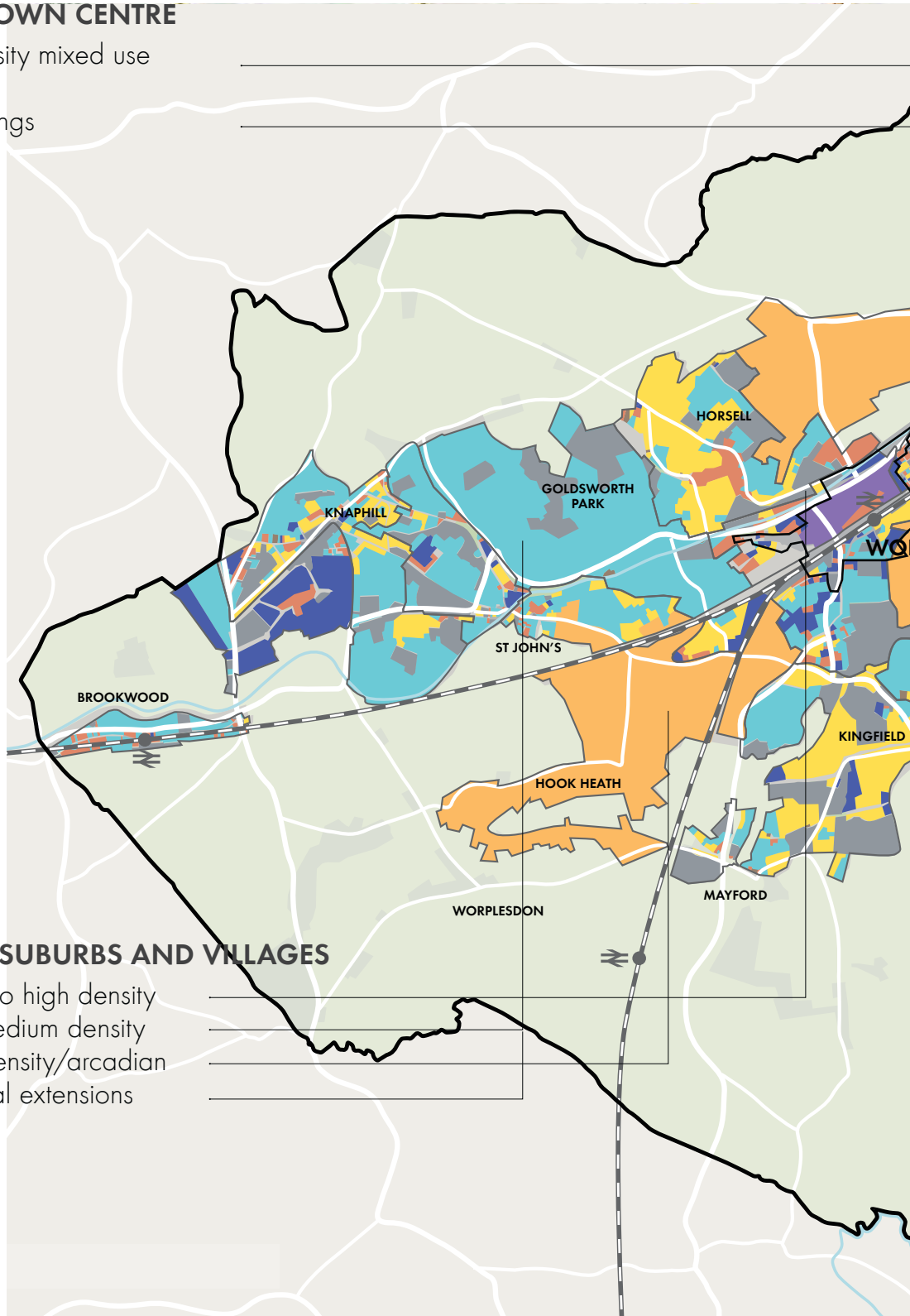
In each case, these categories are illustrated through a possible development scenario and images showing what is and what is not appropriate design. Where certain other issues are particularly relevant to the development scenario, these have also been added as headings.

Each section ends with a good design checklist intended to aid developers, applicants and Council officers in ensuring proposals cover the main themes discussed in that section.



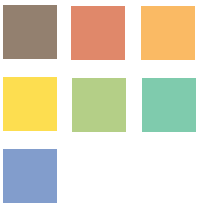
WOKING TOWN CENTRE

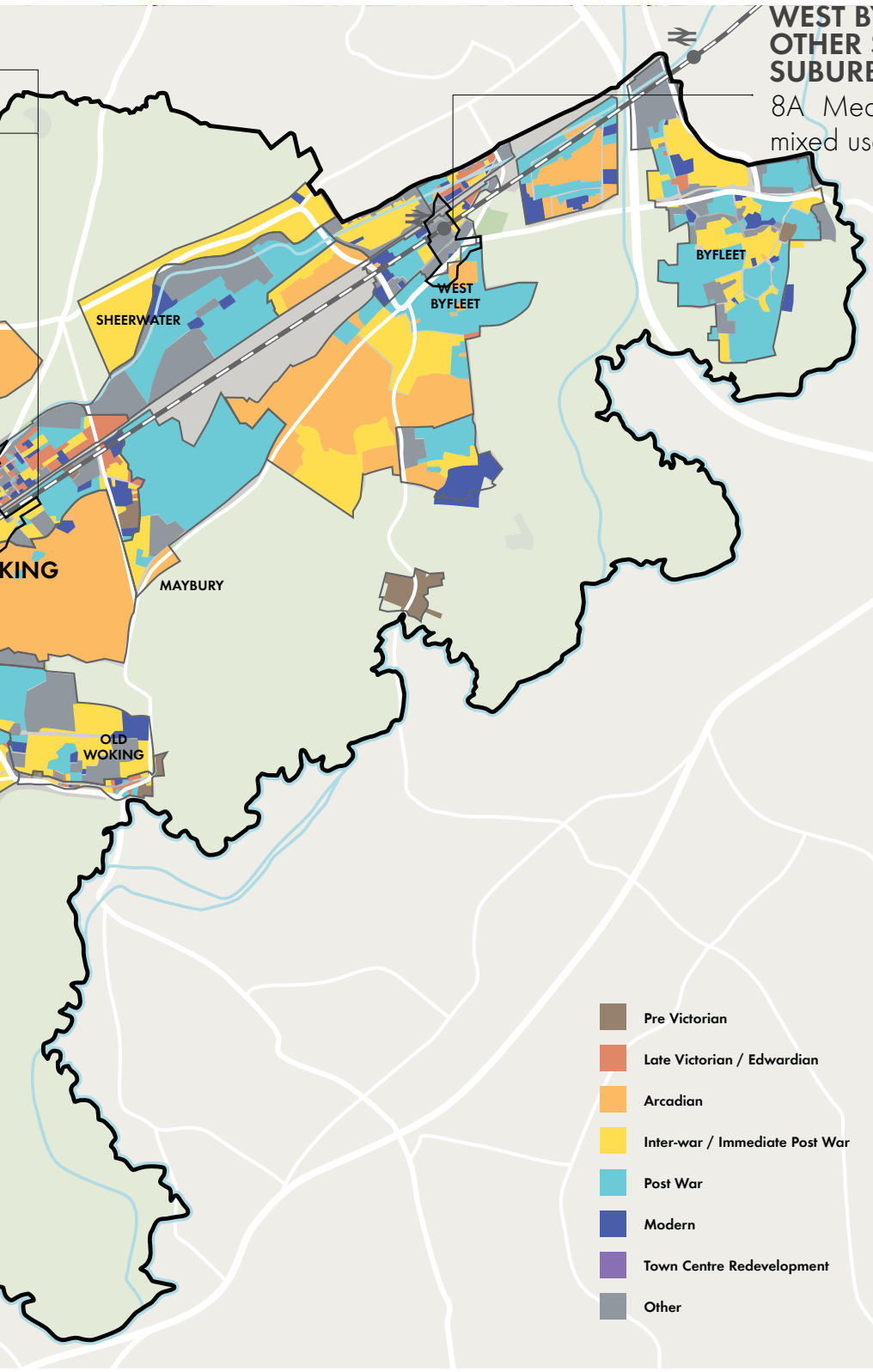
- 7A High density mixed use development
- 7B Tall buildings



BOROUGH SUBURBS AND VILLAGES

- 9A Medium to high density
- 9B Low to medium density
- 9C Lowest density/arcadian
- 9D Residential extensions





WEST BYFLEET AND OTHER SMALLER SUBURBAN CENTRES

8A Medium to high density mixed use development



- Pre Victorian
- Late Victorian / Edwardian
- Arcadian
- Inter-war / Immediate Post War
- Post War
- Modern
- Town Centre Redevelopment
- Other

7 WOKING TOWN CENTRE

7A. High density mixed-use development 7B. Tall buildings

7.1 CONTEXT SUMMARY

Key aspects of Woking Town centre urban design context are summarised here. For further relevant guidance refer to:

- Appendix A1, Woking Design SPD - Woking town centre analysis
- Supplementary policies CS2 Woking Town Centre, CS21 Design and CS24 Woking's landscape and townscape

Historical evolution and assets

The town centre has a strong underlying structure based on a network of historic routes. However, during the 1960s and 1970s much of the centre was substantially redeveloped, which resulted in the loss of historic fabric. Since the 1980s large scale buildings have continued to dominate with little frontage to the public realm. Remaining historic fabric is found in the two conservation areas along the Canal and Chertsey Road.

Character

Woking town centre is dominated by large developments of relatively low architectural quality, which offer little in terms of local distinctiveness. Finer grain areas around the edges of the core offer a more attractive character. Settings of surviving historic buildings need enhancement.

Importance of the public realm

The town centre core has no green spaces and very few hard open spaces or squares. There are also few street trees. However, a coordinated programme of improvements to key central streets is currently being implemented including the completion of Commercial Way, where new street furniture, lighting and trees have been introduced.

Need for better connections

Development over the last 50 years has resulted in much of Woking town centre consolidating into larger urban blocks. Better connections from the town centre core out to the fringe and

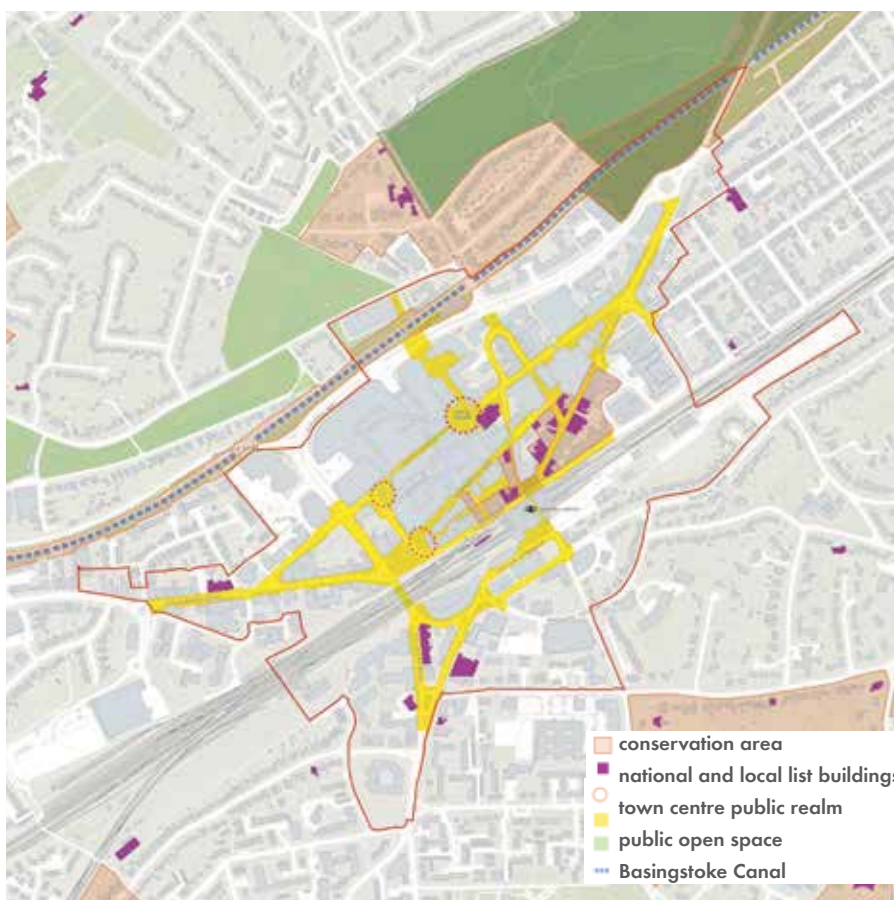
on to the residential hinterland would benefit the town. In particular, better links to the attractive and large open space resource along the Canal would have a major impact.

Structure and legibility

As major developments come forward, opportunities exist to restructure some parts of the town centre to deliver a more cohesive and legible experience for pedestrians.



Woking's historic street network



Woking town centre - highlighting public realm, open spaces, green space, the canal, conservation areas and nationally and locally listed buildings

Assets



Christ Church



High quality historic frontages



The canal corridor



Fine grain town centre edges



The Lightbox



WWF Headquarters



Chertsey Road

Opportunities



Lack of green space



Poor quality streetscapes



New developments need to be of higher architectural quality



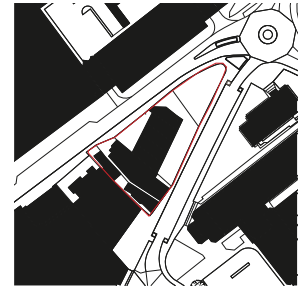
Considerable amounts of degenerated built form



Lack of identity



Need for better quality public spaces



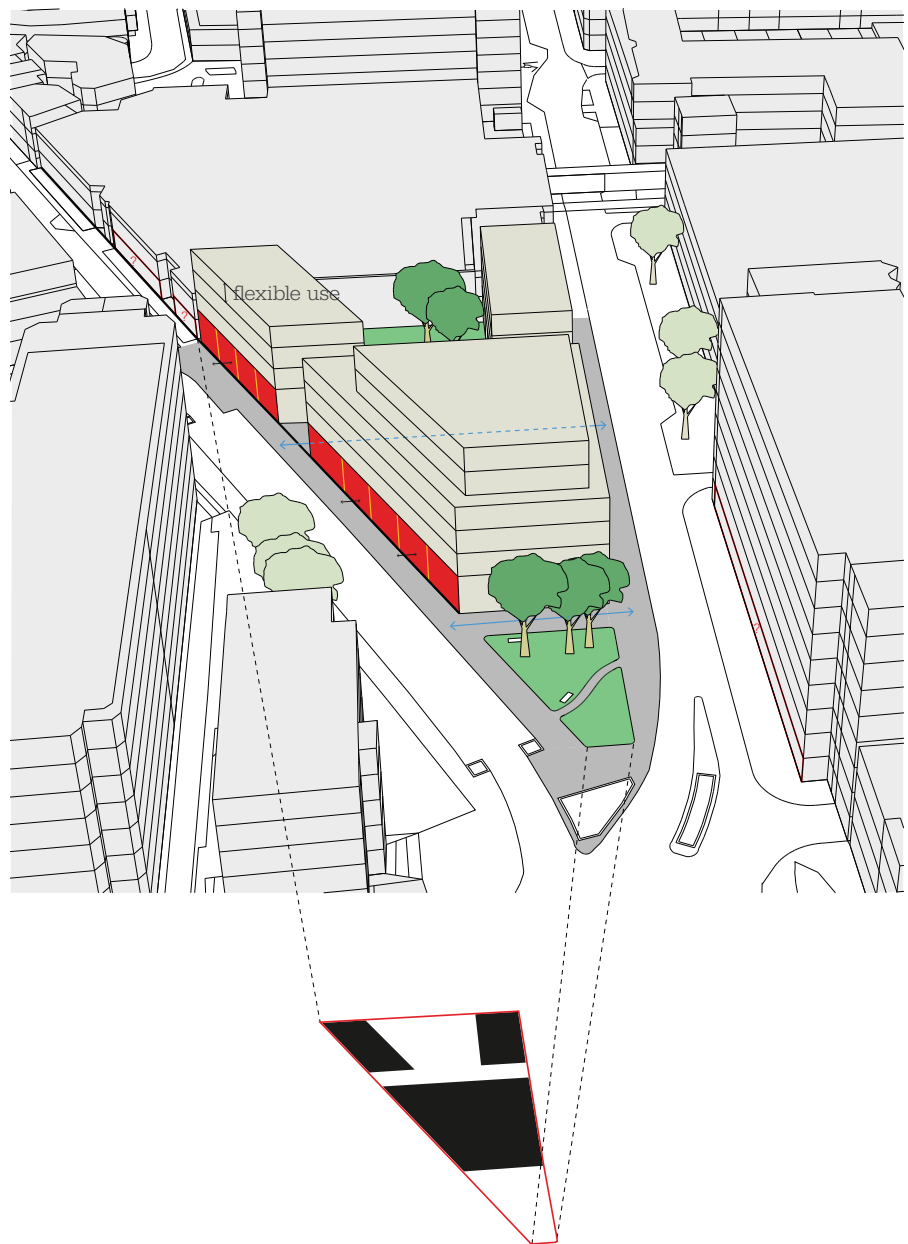
Woking Town Centre example site - existing

7A. HIGH DENSITY MIXED USE DEVELOPMENT

Layout

New high density mixed use development plays an important role in creating attractive, prosperous and sustainable town centre environments. Design must support the aspiration for vibrant town centres with strongly defined, active streets and high quality public spaces.

- Building footprints should not be oversized and should enable adaptation to other uses over the lifetime of the building.
- Proposals must support a permeable and legible street structure, responding to opportunities to improve and create new routes that are safe and attractive, particularly for pedestrians and cyclists.
- New development must positively address the street, complementing adjacent ground floor uses and providing a strong pavement edge.
- Where possible, private space and servicing should be accommodated within the block. Consider basement levels for servicing and parking and roof tops for private amenity space.
- The mix of uses should reflect local need and enhance the area's role in the town centre.
- A perimeter block format is likely to best suit the design objectives.
- Access to different uses in new development should be taken from the street to add interest and activity.



Woking Town Centre example site - Proposal

Dominant building line maintained

Street trees planted



1

Planters slow traffic and create an attractive green edge

Raised road surface helps communicate pedestrian priority

Street trees

Planters increase privacy at threshold between private interior and public exterior

Ample cycle and seating provision

High quality materials



2



3



4

Scale

Account should be taken of the prevailing urban scale in terms of grain, height and massing to help overall legibility.

- A finer grain scale of development is preferable where this adds to the character of the location.
- Large scale developments should be designed to accommodate a range of uses to enable future adaptability.
- Building heights should take account of local opportunities and constraints. See section 4 Woking Tall Building Strategy.

Streetscape

New developments offer opportunities to enhance the streetscape and the public realm.

- Proposals should provide an appropriate footway width to support street activity.
- Street trees should be introduced where possible.
- Opportunities to enhance existing or create new public spaces should be optimised.
- Cycle parking should be secure and is best provided in places that are overlooked.
- Proposals should use high quality and durable materials.
- Providing places to sit enlivens the public realm (2+3).
- Proposals should limit the impact of light pollution from artificial light on local amenity and sites of nature conservation, particularly tall buildings.

1 Chertsey Road, Woking

2 Wolsey Place, Woking

3 Intelligent use of confined area to create a new public space - The Lightbox, Woking

4 High quality public space delivered by a new development - Jubilee Square

Façades

All elevations should offer visual interest, a resolved elevational form and respond to neighbouring buildings.

- Façades should complement adjoining properties and provide appropriate levels of daylight and privacy for internal environments.
- Façade designs should have a consistent and harmonious architectural language across the entire surface with each aspect considered as both a singular element and a part of the whole (7,8+9).
- Openings should be in proportion with adjacent façades (5).
- The vertical and horizontal organisation of the façades should be ordered with a clear hierarchy.
- High quality, durable materials should be used.
- At ground floor, the façade must provide levels of privacy or overlooking appropriate to the external environment (6).
- Elevations should benefit from depth and richness in their treatment (6).
- Proposals should provide legible, detailed drawings of elevations, including the adjacent street scene, to make clear what the proposals impact. All line drawn elevations should include shadows demonstrating protrusions and recesses.



5

Inadequate consistency in opening type and layout

Composition does not sufficiently complement adjacent property



6

Upper floors of façade are predominantly flat and lacking in depth and richness in detailing

A monotonous, inactive façade at ground floor has a negative impact on the streetscape



7 Buildings have a consistent vertical and horizontal structure with openings in proportion with adjacent façades. The treatment at ground floor is appropriate to streetscape and use:
1. Shopfronts are open to the street 2. Office has raised window heights for greater privacy

- 5 Woking Town Centre
- 6 Woking Town Centre
- 7 Southwark Street façades, London
- 8 Highbury Square façade London
- 9 St Andrews, Bromley-by-Bow



8 Care has been taken with the order and proportion of window openings and high quality materials have been used



9 Deep window reveals add visual richness. Frequent individual front doors provide street activity and are favoured over shared access points

Lawn areas

Seating provision

Private amenity space



10

Places for children to play

Planting beds



11



12



13



14

Amenity

Provision of successful external spaces has social, environmental and economic benefits for new developments and the town centre as a whole.

- Proposals must make optimal use of daylight in the design of exterior spaces.
- New developments should mitigate the impacts of noise pollution.
- Proposals should limit the impact of light pollution from artificial light on local amenity and sites of nature conservation, particularly tall buildings.
- Woking town centre is in need of more green spaces and major development proposals should address this deficiency (14).
- In the design of private outdoor space, provision of roof patios, terraces and community spaces should be considered in lieu of conventional private gardens (13).
- Shared amenity space should accommodate a range of uses to provide variety (10).
- The Council may require new developments to contribute to creating or improving areas of public realm in place of any amenity space required by the development where there is limited space to provide it on site.

Climate change

- Planting and, in particular, street trees can provide natural cooling and should be provided where possible (11).
- Facades, windows and doors, should be designed to manage sunlight (12).
- Green roofs and terraces can make significant contributions to biodiversity and also provide opportunities for shared and private amenity space (13).

10 St Andrews, Bromley-by-Bow
 11 Street trees provide natural cooling - Victoria Way, Woking
 12 Roof design manages sunlight - WWF Headquarters, Woking
 13 Greenroofs - Bromley-by-Bow
 14 One of the few usable public green spaces that currently exist in Woking town centre - Church Street East

Access & servicing

Access and servicing requirements must be incorporated into the design from the outset as they can have a significant impact on the surrounding environment.

- Entrances should be given appropriate levels of privacy or openness to suit the type of housing/use proposed and the adjacent external environment.
- All entrances should have step free access at ground floor, with inclusive access to all levels (1.5).
- A coherent servicing strategy should be prepared and submitted with development proposals.
- Where possible, servicing should be accommodated within the block.



15



16



17



18

15 Step free access example

16 Vehicular/service entrance dominates streetscape, Chertsey Road, Woking

17 WWF Headquarters bikeshed, Woking

18 Commercial Way cycle parking, Woking



19



20



21

Parking

The design of parking provision for new developments must be carefully considered from the outset.

- Large service yards or parking areas should not be visible from the street and where possible should be accommodated within the block (16).
- Cycle parking should be secure, often best provided in places that are overlooked, and its location and design should be appropriate to the surrounding environment (17+18).
- Basement parking is encouraged in town centres.
- Car park entrances should be integrated into the fabric of the building, clearly readable to help orientation but not become a visible focus (20).
- The design of parking provision must include adequate space for access, sight lines, turning and manoeuvring.
- Planting and, in particular, street trees help to create more attractive surface parking solutions.
- Where there is sufficient space, on-street parking can help provide a more generous street width and creates a threshold space between vehicular and pedestrian zones (19+21).
- For guidance on parking in non-residential developments refer to the Woking Parking Standards SPD.

19 Effective on street parking example
 20 Car park entrance is integrated into the fabric of the buildings and clearly readable without becoming a visible focus
 21 Use of high quality materials, street trees and clearly defined zones provide an attractive on street parking solution - Elizabeth Street, Belgravia London

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Is the proposal's footprint appropriate to the size of the plot?
- ✓ Does the layout enable pedestrian and cyclist permeability?
- ✓ Does the proposal obstruct existing routes? If so, is this justified?
- ✓ What is each edge of the building addressing and is its treatment appropriate to that condition?
- ✓ Are the building's uses located correctly in relation to the external environment and is the choice of uses appropriate for this location?
- ✓ Does the proposal respect the amenities of neighbouring properties?
- ✓ Is there adequate vehicular and cycle parking and is it suitably designed?
- ✓ Are interior layouts well planned?
- ✓ Is there sufficient amenity space incorporated into the scheme and is its location and design well considered?
- ✓ Have opportunities to provide new or improve existing public spaces been optimised?
- ✓ Is the height of the proposal suitable for this part of Woking town centre?
- ✓ Does the grain of the block, particularly at ground floor, suit its location?
- ✓ Do the facades appear well designed with good attention to detail?

7B. TALL BUILDINGS

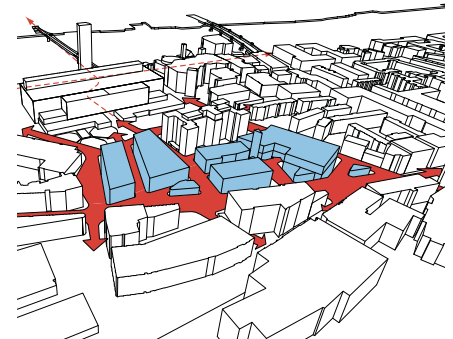
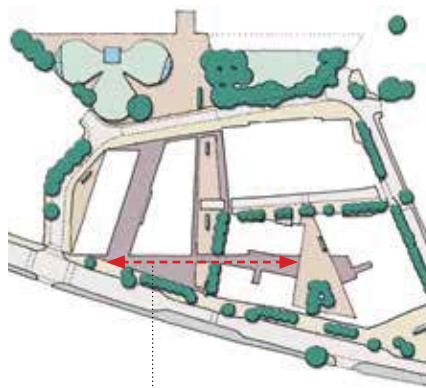
This section should be read in conjunction with Part B - Tall Buildings, which includes Woking's Tall Building Strategy.

All proposals must be supported by a detailed justification, which carefully examines the visual and environmental impact of new tall buildings. All tall building proposals should also follow the advice of the previous section, 7A High Density Mixed Use Development.

Layout

Proposals for tall buildings require the highest design standards with specific attention given to the edges of the building at ground level and the environment that is created.

- The ground floor spaces of the building should be appropriate to the streets and spaces they address (2).
- When establishing the building's footprint, tall buildings usually require greater space to be allocated to surrounding footways and public realm (2).



2

Creating covered public walkways through urban blocks can ensure permeability is not sacrificed at the cost of building mass and capacity

1 How tall buildings address the pavement level is critical to their success
2 The impact of proposed tall buildings should be explored carefully

- Tall buildings are often able to better contribute to the permeability of the town than equivalent lower rise proposals as they enable smaller footprints to be achieved (3).



4

Scale

Whilst the scale of a new tall building will by definition contrast with surrounding buildings, proposals must make a positive contribution to the townscape and skyline and help improve the legibility of the town.

- Proposals for tall buildings must demonstrate their effect on the historic context of the town centre and applicants should supply imagery indicating their visual impact from key locations (4+5).
- Proposals should have a positive relationship with topographical features and other tall buildings, with the virtue of clusters, as perceived from all directions, taken into consideration.



5

Streetscape

Having more breathing space at ground floor creates greater possibilities for new public spaces.

- Setting back the building volume at ground level enables tall buildings to touch the ground more lightly and provides places for shelter (6).
- Careful use of seating elements and street trees helps to define human scale (7+8).
- Development proposals for new tall buildings should exploit opportunities for the improvement of an existing or creation of a new public space (8).



6

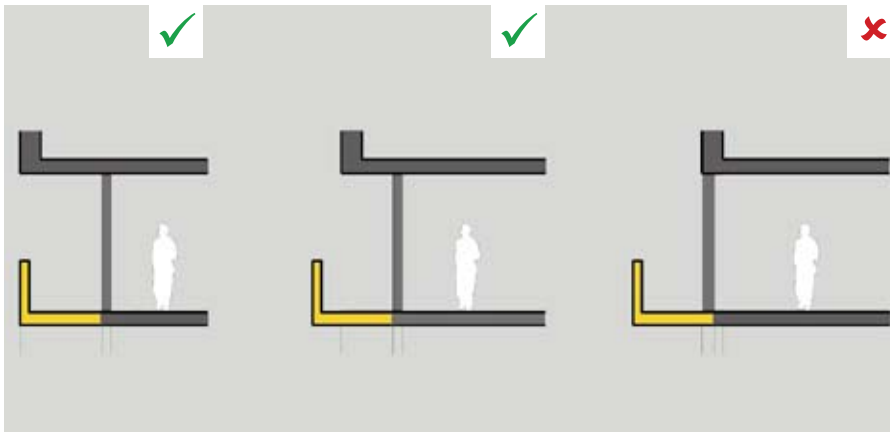


7



8

- 4+5 Woking Town Centre Skyline and Chertsey Road, example images indicating tall building visual impact
- 6 Tall building set backs on Guildford Road, Woking
- 7 Good quality landscaping on Guildford Road, Woking
- 8 New tall buildings can create opportunities for new public spaces in central locations - 30 St Mary Axe, City of London



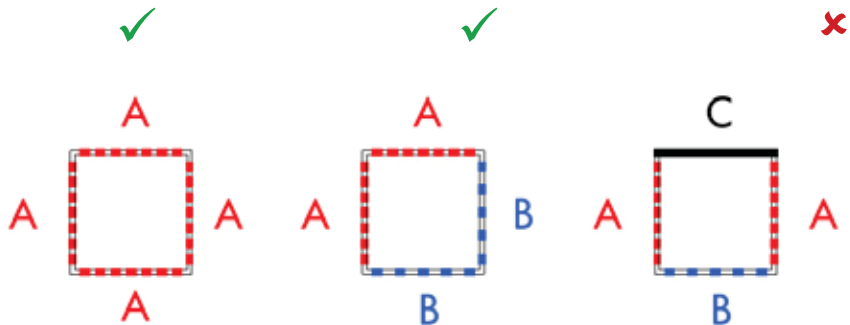
9



10



11



12

Amenity

- Location of private outdoor space should be resolved in terms of its benefit to the individual unit and the whole building composition.
- Balconies in tall buildings should be recessed or semi-recessed to achieve a calm expression, give wind-protection and minimise the risk of bird strike (9+10).

Façades

- As tall buildings are visible from all sides, every façade should be treated as 'front', not 'back' or 'side' (12).
- Each elevation should respond to its orientation, surroundings and aspect (10+12).
- All façade materials should be of consistent high quality and not formed of flat metal panels or large panes of glass but should have depth and definition (10+11).
- Material choices should be informed by long term maintenance considerations.
- All elements should be integrated and part of the overall façade composition to avoid "cluttering".

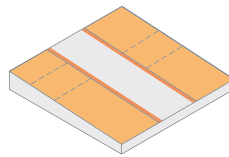
9 Diagram depicting appropriate tall building balconies
 10 Tall Building façade proposal, West Hendon
 11 Façade on Church Street East, Woking
 12 Diagram demonstrating elevational treatment for tall buildings

Environmental Impact

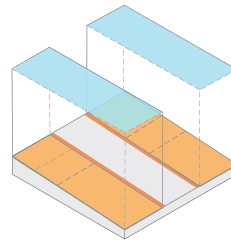
- Proposals for tall buildings must produce wind, shadowing and reflection analysis.
- Setbacks in upper storeys can mitigate overshadowing of external environments and increase daylight levels of lower storeys (13).
- Specific design responses to environmental matters (such as acoustic attenuation, sun-shading, vent elements etc) should be discreetly integrated within the facade to minimise visibility.

Access /Parking

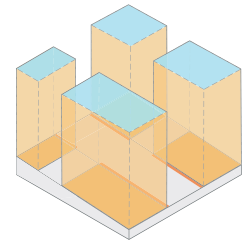
- The main entrance must have a presence and direct access onto the public realm (14+15).
- Access to integral parking provision and service areas should be discreet and subservient to the pedestrian environment.
- Ground floor spaces and units should be generous in height.



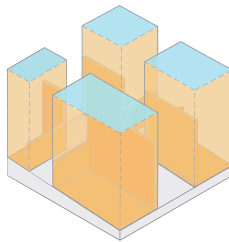
1 Development Zones and Plot Boundaries



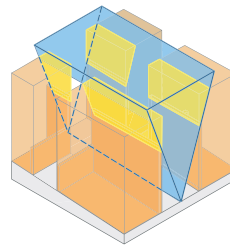
2 Maximum height limits overall massing



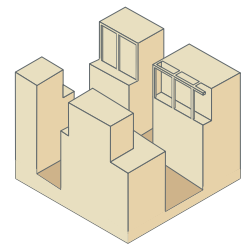
3 Building lines define edge of public realm



4 Elevation height defines enclosure of space and continuity between buildings



5 Setbacks - daylight cones encourage good daylight to buildings and public space



6 Illustrative build-out scheme sample

13



14



15

- 13 Tall Building massing considerations for Kings Cross, London
- 14 Tall Building entrance example, Arsenal Stadium, Allies and Morrison Architects
- 15 Tall Building entrance example, Fitzwilliam College, Allies and Morrison Architects

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Is the height proposed justified for this location?
- ✓ Is the ground level appropriately designed in terms of built form and the surrounding landscape environment?
- ✓ Has the development's visual impact been well demonstrated and does it appear to make a positive contribution to the skyline?
- ✓ Does the proposal make clear its environmental impact in terms of wind tunnels, daylight levels and overshadowing?
- ✓ Does the scheme adhere to all points listed in the good design checklist for 7A?

8 WEST BYFLEET AND OTHER SMALLER SUBURBAN CENTRES

8A. Medium to high density mixed-use development

8.1 CONTEXT

West Byfleet is the largest suburban centre in Woking and therefore is looked at in the most detail in this section. Other suburban centres include Knaphill High Street, Horsell High Street, Brookwood Connaught Road, Sheerwater Dartmouth Avenue, Old Woking High Street, Maybury Hill/Maybury Road, Goldsworth Park, Goldsworth House and St John's. All new development in these areas should acknowledge the guidance provided here. For further guidance refer to:

- Appendix A2, Woking Design SPD - West Byfleet analysis
- Supplementary policies CS21 Design and CS24 Woking's landscape and townscape

8.2 WEST BYFLEET

History and Character

- Developed as a commuter suburb following the opening of the station in 1887. Much of the residential building is from this era.
- Finer grain and lower scale than Woking town centre.
- Mostly an inter war style layered on top of a village format.
- Historic character principally found on Old Woking Road and Station Approach.
- The scale of Sheer House is at odds with the prevailing scale and character.



1914



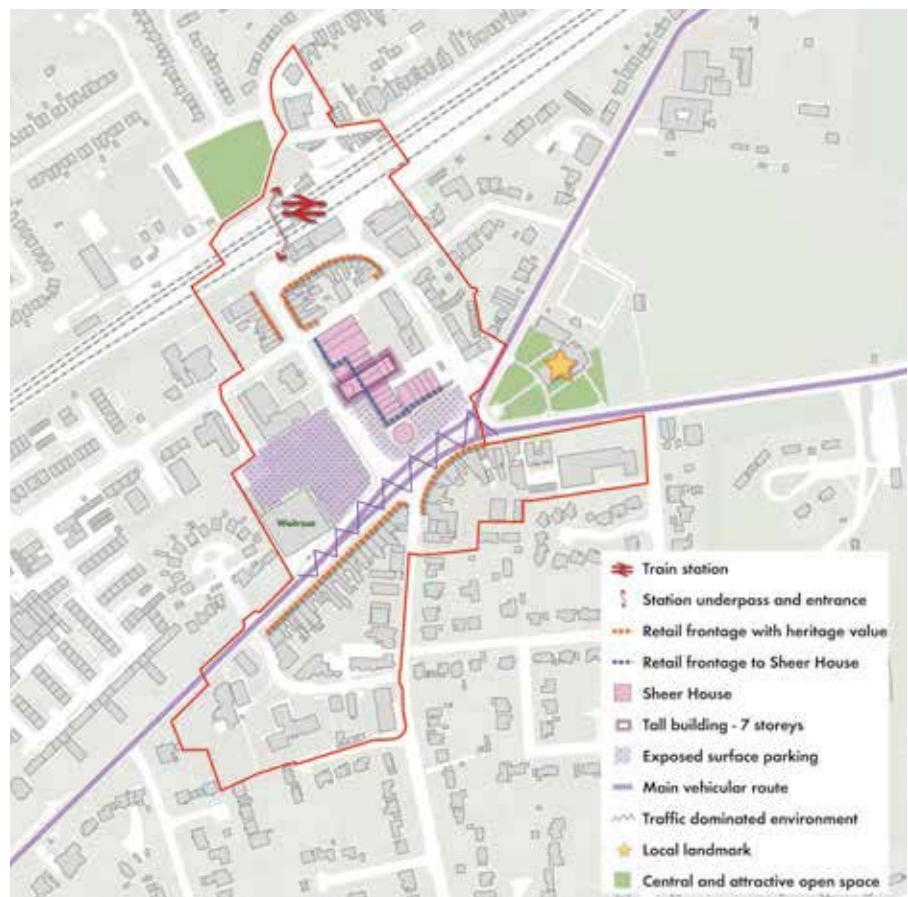
1965

Scope for new and improved public spaces

- Evolution of the centre has led to removal of public spaces and quality deteriorating.
- Better linking existing spaces such as the village green to the north of the station, Church Gardens into the centre and to provide new public space would significantly improve the centre.

Opportunity for a more cohesive district centre

- The centre is currently divided into several parts - the station and Station Approach, Old Woking Road area and the small pocket of town centre north of the station.
- Delivering improvements which better connect these disjointed environments would deliver significant improvements.
- With residential intensification and growth in the surrounding hinterland there is potential for further investment in the centre to strengthen its economic vitality.



West Byfleet Opportunities Identified

Assets



Village character



Heritage



Green edges



Active, wide pavements



St John's Church



Independent shops on Old Woking Road



Street trees

Opportunities



Prominent servicing/parking areas create poor quality streets



Convoluted access points



Landscape shows vehicular priority



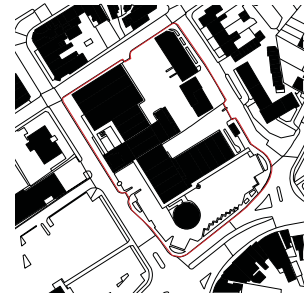
Green space disconnected from centre



Run down built form



Car dominated public realm



Sheer House site - existing

8A. MEDIUM TO HIGH DENSITY MIXED-USE DEVELOPMENT

Layout

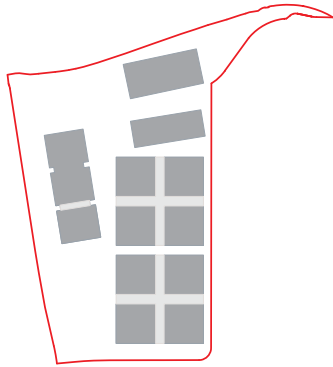
New development in suburban centres should respect the historic form of the area with buildings addressing a high quality streetscape with wide footways and street trees.

In West Byfleet, the Sheer House site occupies a significant proportion of the district centre and, in its current form, has a negative impact on the character of the area. It is used here as a case study to explain the issues and opportunities faced in suburban centres (1).

- Where possible, proposals should aim to re-establish a perimeter block format enabling a clear definition between front and back.
- Building footprints should not be oversized and should enable adaptation to other uses over the lifetime of the building.
- Private space and servicing should not address a public streetscape.
- New development should take account of any opportunities to provide new pedestrian public space.
- Proposed uses should reflect local need and enhance the designated centre.
- Proposals should not diminish current vehicular parking numbers.



1 Indicative potential layout of new development at Sheer House. The layout shows opportunities for a more permeable structure with increased public space including wide footways and street trees, more activity at street level and a more appropriate scale of development.



Establish a clear block structure that is not oversized and ensures pedestrian permeability
2



Deliver new amenity spaces such as wide footways, public spaces and private courtyards and gardens



3



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Scale

Woking's suburban centres range from 3-4 storeys in height in West Byfleet and 1-3 storeys in other smaller centres.

- The height of Sheer House in West Byfleet is inappropriate for the centre. New proposals should accommodate medium scale buildings (c.5-6 storeys) in a clear block structure with good public space.
- A finer grain scale of development is preferable, in keeping with the current character.

Streetscape

New development offers opportunities to enhance the streetscape and the public realm.

- Proposals should have appropriately wide footways that are in keeping with the historic street layout (3,4,5+6).
- Street trees and planting should be introduced where possible to maintain the area's strong green character (3,4,5+6).
- Opportunities to enhance existing or create new pedestrian public spaces should be reviewed and, where possible, taken.
- Boundary treatment should be in keeping with adjacent properties and appropriate to the immediate streetscape condition (3,4,5+6).
- Provision for cycle parking should be implemented where possible.
- Opportunities to increase pedestrian priority on and around the Sheer House site should be considered.

Max. 6 storeys

Terraces provide alternative private amenity spaces

Parking interspersed with trees

Wide pavements maintained

Raised table communicates pedestrian priority



Green roofs

Front gardens provide privacy

Discreet access to parking

Flexible ground floor with active frontage

- 2 Example proposal demonstrating layout considerations
- 3 Lack of landscaping and poor quality shop fronts undermine station-side development of an appropriate scale - Station Approach, West Byfleet
- 4 Poor quality retail forecourts undermine the quality of the town centre environment - Parvis Road, West Byfleet
- 5+6 Good quality, well proportioned shopfronts, with generous pavements and street trees, create successful shopping environments - Station Approach and Old Woking Road, West Byfleet and High Street, Knaphill
- 7 Mixed use redevelopment - worked example

7

Amenity

Provision of successful external spaces has social, economic and environmental benefits for new developments and town centres as a whole.

- Proposals should help better connect existing green spaces in the town centre (8).
- Opportunities to provide new pedestrian public spaces should be enhanced, particularly on the Sheer House site (8).
- New public spaces should be designed using high quality, hard-wearing materials.
- Provision of private amenity space needs to be carefully considered. Roof patios, terraces and community spaces could be implemented in lieu of conventional private gardens (10).



8

Façades

The design of new façades should be sensitive to the historic buildings found in the surrounding context (9+10).

- Façades should complement adjoining properties and provide appropriate levels of daylight and privacy for internal environments.
- The vertical and horizontal organisation of the façade should be ordered with a clear hierarchy.
- High quality materials should be used.
- Detailing must be well considered with high-quality solutions sought.
- At ground floor the façade must provide levels of privacy or overlooking appropriate to the external environment.
- Proposals should provide legible, detailed drawings of elevations, and preferably renders, to make clear what the proposals look like. All line drawn elevations should include shadows.



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8 Opportunities to connect existing public open spaces in West Byfleet

9+10 Typical historic façades, West Byfleet

11 Balcony and rooftop private amenity space has been designed into the scheme from the outset

12 Poor quality detailing and private amenity space

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Is the proposal's footprint appropriate to the size of the plot?
- ✓ Is the height of the development suitable for this location?
- ✓ Is the level of pedestrian and cyclist permeability in the area maintained, or preferably enhanced, by this scheme?
- ✓ Have opportunities to provide new or improve existing public spaces been optimised?
- ✓ Does the proposal demonstrate an acknowledgement of the historic form, green character and generous streetscapes typical of Woking's suburban centres?
- ✓ What is each edge of the building fronting and is its facade design and boundary treatment appropriate to that condition?
- ✓ Are the building's uses located correctly in relation to the external environment and is the choice of uses appropriate for this location?
- ✓ Does the proposal respect the amenities of neighbouring properties?
- ✓ Is there adequate vehicular and cycle parking and is it suitably designed?
- ✓ Are interior layouts well planned?
- ✓ Is there sufficient amenity space incorporated into the scheme and is its location and design well considered?
- ✓ Do the facades appear well designed with a sensitive approach to neighbouring buildings and good attention to detail?

9 BOROUGH SUBURBS AND VILLAGES

- 9A. Medium to high density
- 9B. Low to medium density
- 9C. Lowest density (Arcadian)
- 9D. Residential extensions

9.1 CONTEXT

Range of characters

- Large range of residential characters and varying design quality.
- Loss of local distinctiveness in some post war and modern development.

Street character

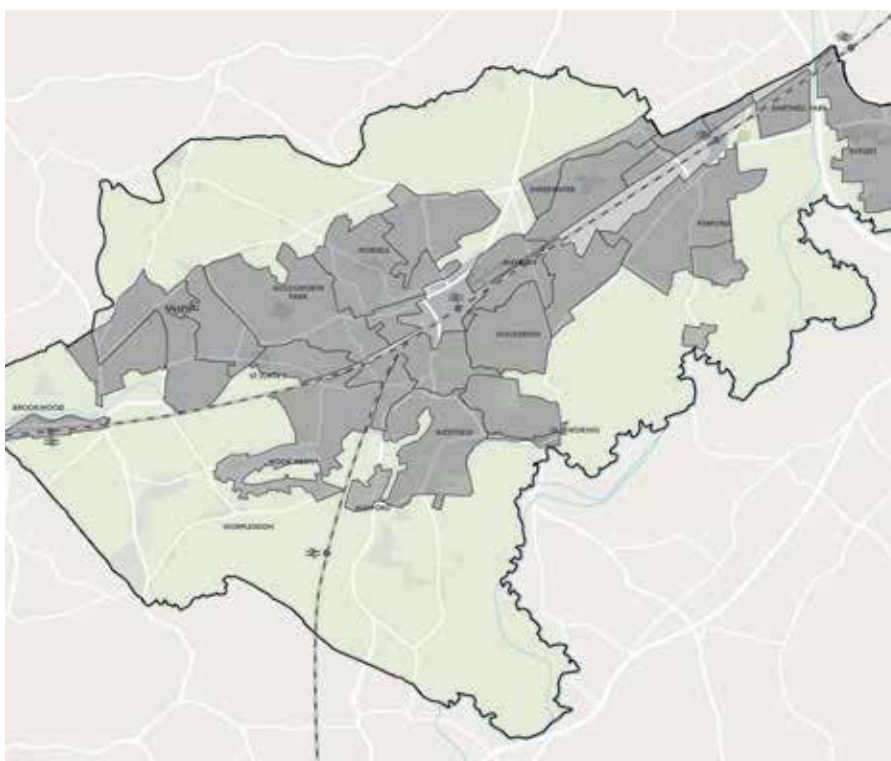
- The more attractive suburban areas are defined by streets with a strong green character with front gardens, hedge lines, street trees and verges.
- The quality and clarity of boundary treatments has a strong impact on the streetscape.
- Areas where parking has not been properly planned tend to be less attractive.

Public realm/open space

- Need to ensure public space provision functions for all residents.
- Good play provision, ample recreation space, attractive places for walking and relaxation and enhanced landscape are essential.

Supporting longer term sustainability

- Many of the suburban areas in Woking are car-orientated. There is a need to provide additional transport choices in these neighbourhoods to encourage long term sustainability.



Suburban neighbourhoods comprise a significant proportion of the borough



Hook Heath - Highly attractive historic assets



Goldsworth Park - Cul-de-sac developments have characterised much of the post war development

Assets



Highly valued residential neighbourhoods



Diversity in housing types and styles



Extensive green street character



Good quality boundary treatments and street trees

Opportunities



Cumulative impact of alterations can be harmful



Blank edges and left over land



Loss of front gardens to parking



Loss of clear boundary treatments



Parking requirements for new houses can result in loss of front gardens and expansive cross-overs



Some modern development has disrupted the relationship between building and street



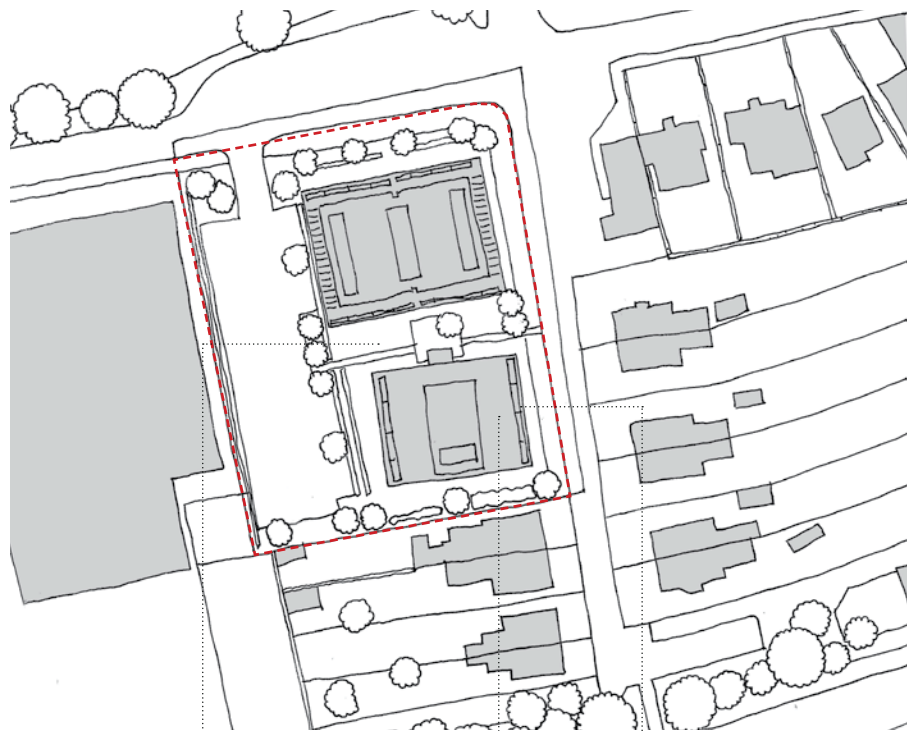
Example site - Existing

9A. MEDIUM TO HIGH DENSITY (30-70 DPH)

Layout

Adjacent to town and district centre boundaries there may be a number of opportunities for redevelopment of existing sites where it is appropriate to consider housing densities higher than surrounding and adjacent suburban areas. Such proposals need to be well-considered and carefully designed to respect the surrounding context.

- A perimeter block format is likely to be the most appropriate form to ensure a good relationship to surrounding streets.
- In many cases the redevelopment will offer an opportunity to increase local permeability. A clear internal street network should be delivered supporting connections across the site.
- The mix of uses and layout of blocks must be sensitive to relationships with adjacent uses and features, particularly nonresidential uses and landscape features such as watercourses.
- Private space and servicing should be accommodated within the block where possible.

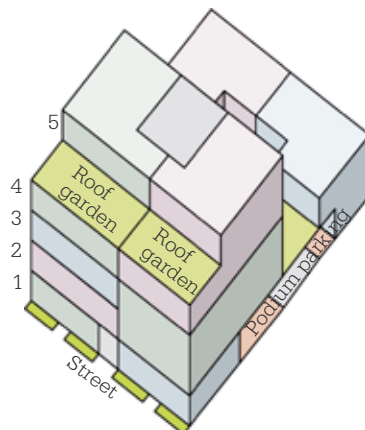


Opening up of routes into the site increases local permeability

Footprint of new development introduces new typology to the area, reducing previous industrial scale to something more sympathetic to a residential neighbourhood

Front doors on to the street are maximised to reinforce the development's positive relationship with the street

Example site - Proposal



Scale

Increasing the density of development on the site must be managed sensitively.

- The scale must be appropriate to the context and overall legibility.

1 Good example of a medium density courtyard-based development - Chapel Road development, Southampton
2 CABE library, National Archives



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- Building heights should reflect the prevailing local context and is unlikely to exceed five storeys.
- Areas tend to be within a 25-70 dwellings per hectare density range.

Streetscape

Redevelopment of sites to deliver greater capacity means even greater attention is needed to produce successful street environments.

- Existing building lines should be continued or restored.
- Within and around the development wide footways should be accommodated to deliver a comfortable and accessible pedestrian environment (3).
- Street trees should be well accommodated.
- Every unit should directly address the street with frequent doors along the street frontage to access flats above.

Amenity

The provision of private, shared and public open space within flatted schemes needs careful consideration to ensure it meets residents' needs.

- Public open space standards should be met through a well considered strategy.
- Daylight/sunlight considerations must be explored to avoid overly shading public spaces and adjacent development.
- Ample private outdoor space should be integral to the scheme, and can be provided in a range of formats (4+5).
- The importance of mitigating noise pollution within higher density developments must be recognised and reflected in specifications.

3 Boundary treatments and front gardens combine to reinforce a green street character - Accordia development, Cambridge

4 Providing private and communal outdoor space in a variety of forms - Accordia development, Cambridge

5 Well proportioned balconies provide usable private outdoor space - Accordia development, Cambridge

Façades

Elevations should have a consistent and harmonious architectural language across their entire surface with each aspect considered as both a singular element and a part of the whole.

- The design should demonstrate an appropriate relationship to adjacent properties, taking account of prevailing storey heights and roof lines.
- The vertical and horizontal organisation of the façade should be ordered with a clear hierarchy.
- Detailing must be well considered with high-quality solutions sort (7).
- Drain pipes should be accommodated appropriately in the composition - not added on as an afterthought (8).
- The appearance of new developments should respect the that of existing properties. This does not mean it has to directly copy the look of them but the existing grain, massing and boundary treatment should be acknowledged (9).



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Access / parking

Design of parking should be incorporated into the scheme from the outset to ensure well considered solutions are achieved.

- Provision of parking must achieve an appropriate balance between reducing dependency on the car whilst ensuring parking demand does not adversely affect surrounding streets.
- Servicing and vehicle access should be logically accommodated and not cause conflict with the pedestrian experience.



10

7 Generous windows and servicing are well integrated - Gresham Mill

8 Better integration of rainwater management would have reduced facade clutter - Heathside Crescent

9 Facades at Ijburg, The Netherlands, are contemporary whilst respecting traditional grain, massing and boundary treatment

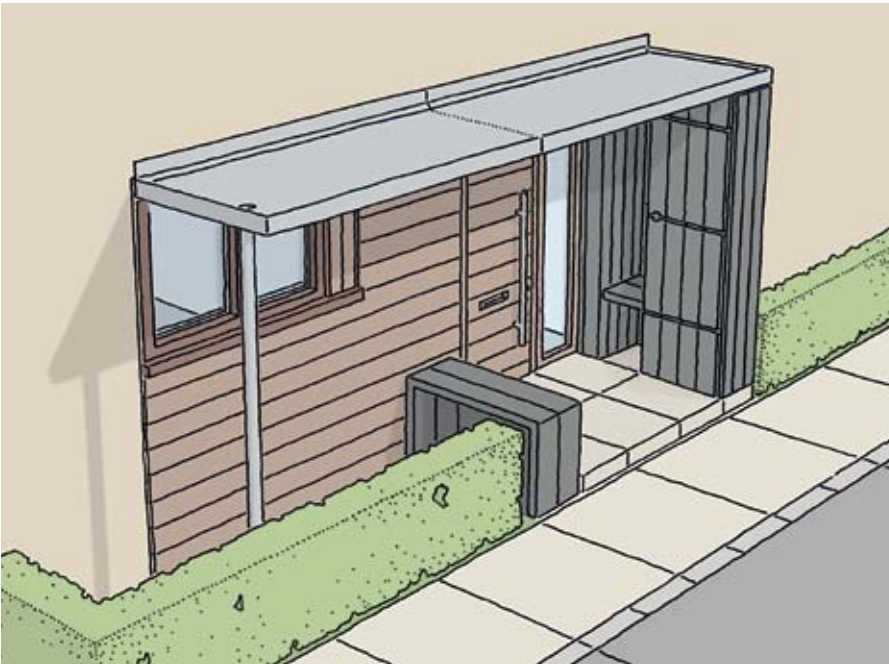
10 Deck parking at Nieuw Terbregge, The Netherlands, separates car traffic from pedestrians and provides residents with a large shared amenity space



11



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13 Specific, integrated storage for bins and cycles next to entrance reduces street clutter

- Basement and deck parking should be considered alongside a combination of on-street, courtyard, integral and on-plot parking to accommodate appropriate levels of parking on new developments.
- Parking standards must be adhered to.
- All entrances should have step free access at ground floor, with inclusive access to all levels.

Climate change

- Facades, windows and doors should be designed to manage sunlight appropriately.
- Generous vegetation and street trees should be accommodated to create a strong landscape setting and ensure buildings and spaces are sheltered.
- Roofs and terraces should make an active contribution to biodiversity and landscape character.

External storage

When storage, and particularly bin storage, is not adequately integrated early into a scheme's design it can have a lasting detrimental impact on the impression of a place.

- Space for waste and recycling storage should be integrated into the design of housing to support easy access and minimise street clutter.
- The space need not be fully enclosed, but should work as practical space for storage.
- Where possible the space should be flexible to future changes in bin design.

11 Space for generous planting should be incorporated into the landscape design

12 Example of appropriate communal bin storage - Gresham Mill development

13 Example of well integrated bin storage - Trumpington Meadows, Cambridge

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Is the density proposed justified for this site?
- ✓ If the density is higher than the surrounding context, does the design manage this appropriately?
- ✓ Does the proposal have a well considered approach to the landscaping of the adjacent streetscape and amenity spaces, demonstrating suitable strategies for materials and planting?
- ✓ Is the proposal's footprint appropriate to the size of the plot?
- ✓ Is the level of pedestrian and cyclist permeability in the area maintained, or preferably enhanced, by this scheme?
- ✓ Are the building's uses located correctly in relation to the external environment and is the choice of uses appropriate for this location?
- ✓ What is each edge of the building fronting and is its facade design and boundary treatment appropriate to that condition?
- ✓ Does the proposal respect the amenities of neighbouring properties?
- ✓ Is there adequate vehicular and cycle parking and is it suitably designed?
- ✓ Are interior layouts well planned?
- ✓ Is there sufficient amenity space incorporated into the scheme and is its location and design well considered?
- ✓ Do the facades appear well designed with a sensitive approach to neighbouring buildings and good attention to detail?



Example site - Existing

9B. LOW TO MEDIUM DENSITY (15-45 DPH)

Layout

In areas of existing or planned low to medium density housing, the street network is key to creating successful places.

- The existing structure of a neighbourhood should be respected and any interventions should support or enhance the relationship to the street.
- An appropriate balance of public and private space must be achieved, with clear boundaries and definition between spaces.
- The layout should respond to local topography and landscape assets.
- A clear network of streets and public space should be established.
- New developments should be connected to adjacent housing areas in order to support wider permeability and pedestrian and cycle connections.



Example site - Proposal

Scale

The scale of development should be informed by the prevailing scale of surrounding streets.

- The existing pattern of plot subdivision, building form and footprint should be respected (1).

1 Respecting the fine grain of development found in low to medium density areas allows a range of house types and sizes to be supported

2 Off street parking concealed by appropriate boundary treatment



1



2

- Much of the Borough is characterised by housing of two and three storeys. New developments should respect this scale of development.
- Housing density tends to be 15-45 dwellings per hectare.

Streetscape

New developments must provide sufficient enclosure and should directly address the street.

- A context-led approach to building line is needed.
- Clear and attractive low boundary walls combined with hedges and shrubs can help to maintain a positive relationship between dwellings (3).
- Boundary treatment along the frontage of infill developments should closely match that prevailing in the street scene, particularly where there are continuous hedges. Open frontages will not be permitted in streets where enclosed front boundaries prevail and vice versa
- Street widths should be generous to accommodate pedestrians, cycles, street trees and on street parking where suitable.
- New development should deliver appropriate provision of public open space, in a variety of forms, to suit existing and future needs.
- Where off-street parking is proposed it should not dominate the streetscape.



Established building line and set back respected

Street trees retained and new additions planted

Consistent boundary treatment supports definition of space and street

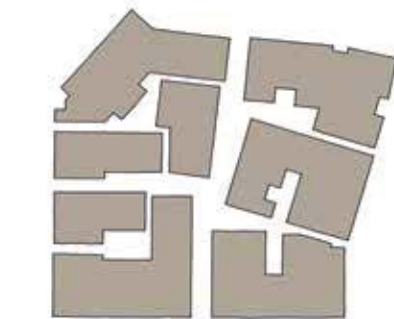


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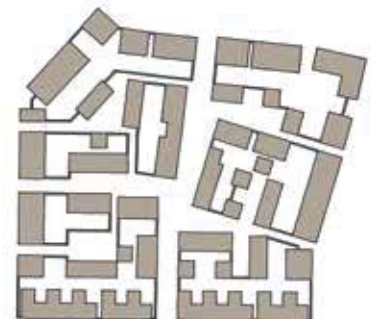
Amenity

The privacy and amenity of homes and gardens should be optimised.

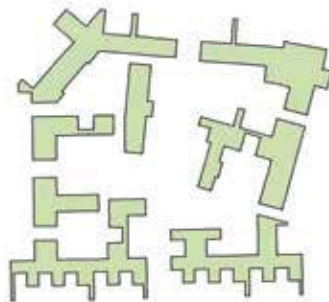
- A comfortable and appropriate relationship to adjoining properties must be established.
- Daylight and distance standards should be observed.
- Front and rear gardens should be provided that are sympathetic to the character of the area, adequate in size and functional.



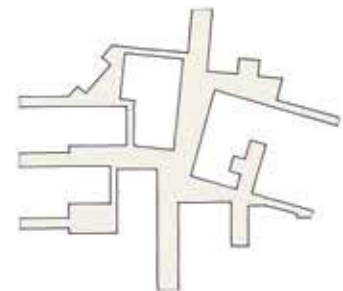
Clear block structure - a flexible urban structure capable of accommodating both formal and informal forms of development.



Buildings arranged in perimeter block formats - building fronts work together to define a strong street edge with less formal arrangements within the block.



Clearly defined and well proportioned private gardens.



Network of public space and streets.

- 3 Appropriate boundary treatment example
- 4 Suitable provision of new public space with a good relationship to new dwellings
- 5 Low-medium density development strategy

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Façades

New development should take precedent from the local historic fabric, using high quality, hard-wearing materials with a careful attention to detail (6).

- Elements such as drainage should be integrated into the design from the outset (8+9).
- Detailing must be well considered with high-quality solutions sort (9).
- Use of contemporary façades can lift an area if designed with respect to context (10).
- The vertical and horizontal arrangement of windows should be well considered and balanced (8).
- Windows should be generous in size.
- The overall composition of façades should have a consistent and harmonious architectural language.
- Prevailing storey heights should be respected.
- Robust and high quality materials should be used.

Climate change

The majority of the built up area of the Borough is characterised by medium density, suburban housing. The way in which new development is incorporated into these areas will have a direct impact on issues associated with climate change.

- 6 Historic façades use high-quality materials and are well detailed.
- 7 Poor detailing creates ugly façades
- 8 Terraced housing façades with large windows, well considered detailing, bin storage and good quality robust materials
- 9 Simple, good quality detailing and composition creates an attractive elevation
- 10 Good contrasting facade example at Cardogan Terrace in Hackney - the traditional boundary treatment, facade proportions and overall massing are in keeping with adjacent properties but the project is realised in a modern aesthetic

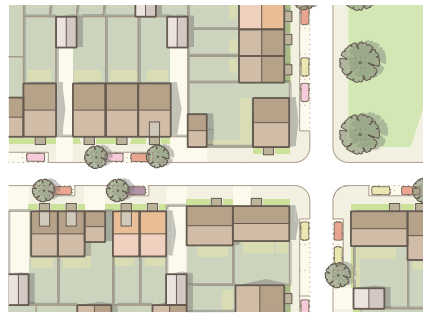


Consistent roof and eaves line ensures a cohesive streetscape

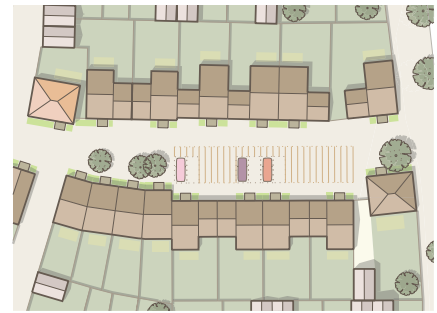
Ratio of solid wall to window is well balanced

Chimneys help to provide a sense of rhythm

- Proposals must acknowledge the importance of vegetation in supporting climate change.
- Using green roofs can make significant contributions to biodiversity and can also provide additional amenity space.
- Drainage, shading and ventilation solutions should be carefully considered.
- Larger developments should take account of opportunities to provide alternative forms of energy production, on site sewage treatment, rainwater recycling systems and natural ventilation.
- Consider using roofscapes and south facing facades for energy generation.
- All solutions should be well integrated into the design of new developments and have no adverse impact on character



Street based parking



Shared surface parking



Courtyard parking



Parking around a square, park or green

11

Access / parking

Incorporating parking into new development is key to the delivery of successful, well designed environments.

- Proposals should have a permeable structure that offers pedestrians a choice of routes.
- Residential streets should have pedestrian and cyclist priority.
- Accessibility standards must be met to support inclusivity.
- Parking should be integrated into the design of streets or plots from the outset (11).
- A combination of on-street, courtyard, integral and on-plot parking should be considered to accommodate appropriate levels of parking on new developments.
- Parking standards must be adhered to.
- Refuse storage must be integrated into the design from the outset to minimise street clutter and support



12



13

easy access (12+13).

- Multiple new access points in the existing road frontage will be discouraged. Where possible, existing access arrangements should be used to maintain the appearance of the street frontage. Consider new dwellings sharing access drives.
- Access drives serving backland sites must be suitably located away from existing dwellings to avoid noise and visual disruption. A minimum separation of 3m is suggested, together with screening by an appropriate boundary treatment to match that of the street frontage.

11 Ensure a range of parking options are considered from the outset

12 Lack of refuse storage provision creates a cluttered streetscape

13 Well-designed bin storage provides easy access and an attractive streetscape edge

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Is the proposal in keeping with the area's current density of 15-45 dwellings per hectare and a general building height of 2 or 3 storeys?
- ✓ Does the proposed street network respect, or preferably enhance, the existing structure and permeability of the neighbourhood with adequate routes for pedestrians and cyclists?
- ✓ If cul-de-sacs are proposed, are there still appropriate levels of connectivity and permeability throughout the development?
- ✓ Does the layout of the development provide individual homes with adequate levels of privacy, amenity space and parking facilities?
- ✓ Are interior layouts well planned?
- ✓ Do the facades appear well designed with a sensitive approach to the surrounding neighbourhood and good attention to detail?
- ✓ Does the proposal have a well considered approach to the landscaping of streetscapes and amenity spaces, demonstrating suitable strategies for materials and planting?



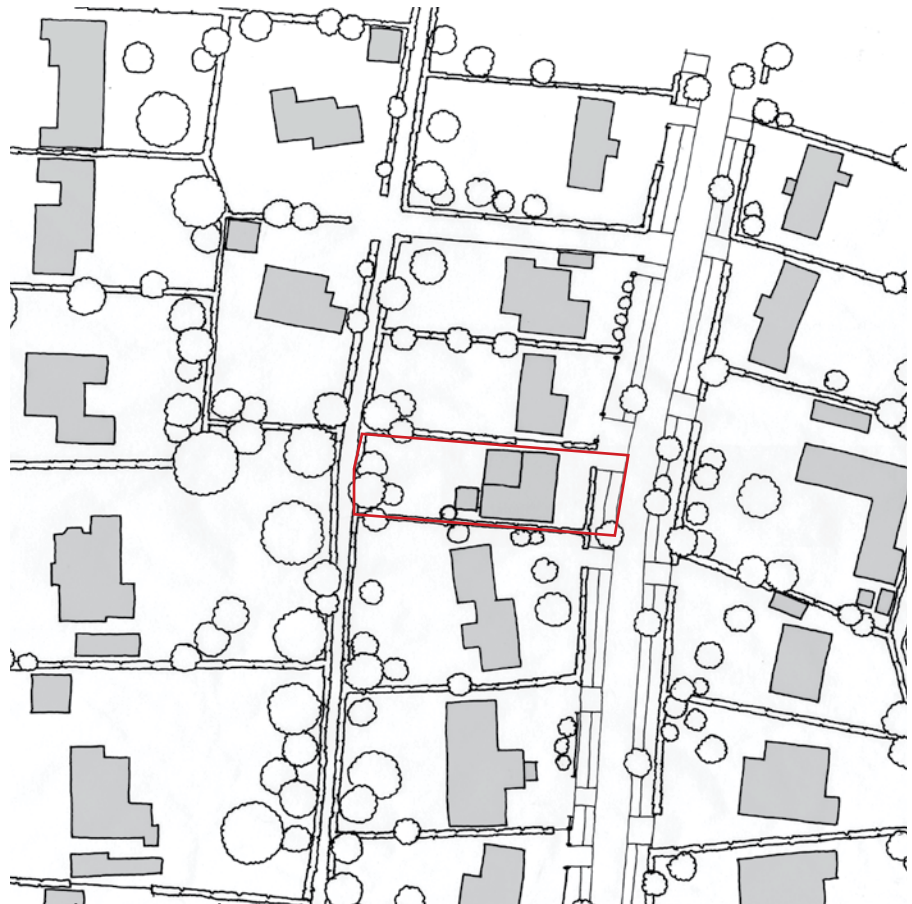
Example site - Existing

9C. LOWEST DENSITY/ARCADIAN (2-15DPH)

Layout

The Arcadian areas of the Borough are typically under 15 dwellings per hectare and found in Hook Heath, The Hockering, Pырford and West Byfleet. They are characterised by large detached properties on generous plots. Neighbourhoods tend to have a strong green character.

- Interventions should respect the picturesque approach to landscape design that has informed the development of these areas whereby detached houses are set in their own landscape surrounded by mature trees.
- These areas require a sensitive response to existing topography and landscape assets.
- Boundary treatments are of upmost importance in defining the relationship between private space and serving street.
- The building footprints and low density provide important parameters for new development.



Example site - Proposal

Scale

Arcadian areas are characterised by their landscape qualities with mature hedges and trees creating lush and attractive streets with proportionally small building footprints and scales.

- Areas tend to be within a density range of 2-15 dwellings per hectare.

1 Established planting in front gardens and boundary treatment determine the character of these streets



1



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- Building height should be governed by local character.

Streetscape

In these areas, the streetscape is dominated by landscape features (2).

- The front gardens of properties usually determine the character and quality of the streetscape.
- Natural edges and boundary treatments have a very important roles to play (5)
- Artificial boundaries, such as high brick walls and fences, will need to take account of the pedestrian environment and be softened with additional planting to maintain prevailing green character (6+7).
- Houses are often well set back with an irregular or indiscernible building line.
- Streets are generally wide with houses benefitting from large front gardens, although verges are often not present.

Amenity

Residential amenity issues may arise where development proposals come forward that intensify the use of land.

- New development should respect the prevailing Arcadian form of development and not introduce more suburban forms of development.
- Large front gardens and secluded housing plots should prevail to help protect the amenity of existing residents.
- The relationship between adjoining gardens and habitable rooms will be a key in protecting the existing residential amenity.

2 Contemporary building fits into its Arcadian context by well designed landscaping and soft, green boundary treatment

3 Off street parking concealed by established boundary planting

4 Planting and soft boundary treatment is in keeping with area's character

5 A combination of gates, brick posts and established hedge provides appropriate levels of privacy whilst maintaining a positive relationship between dwelling and street

6 High fences create streets which lack activity and overlooking

7 A combination of fencing and hedges provides a more successful boundary treatment

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Is the proposal in keeping with the area's current density of 2-15 dwellings per hectare and a height limit of 3 storeys?
- ✓ Does it respect the local picturesque approach to landscape with a strong green character?
- ✓ Are boundary treatments well considered, in particular those that address a public streetscape?
- ✓ Is the building appropriately set back from the street to reinforce the generous street character typical of these parts of Woking?
- ✓ Does the proposal respect the amenities of neighbouring properties?

9D. RESIDENTIAL EXTENSIONS

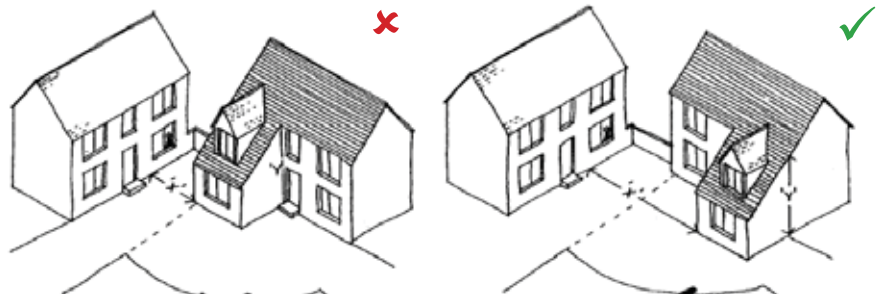
Context

Many residential extensions can be undertaken without planning permission under 'Permitted Development Rights'. Any extension proposals that go beyond these parameters require planning permission.

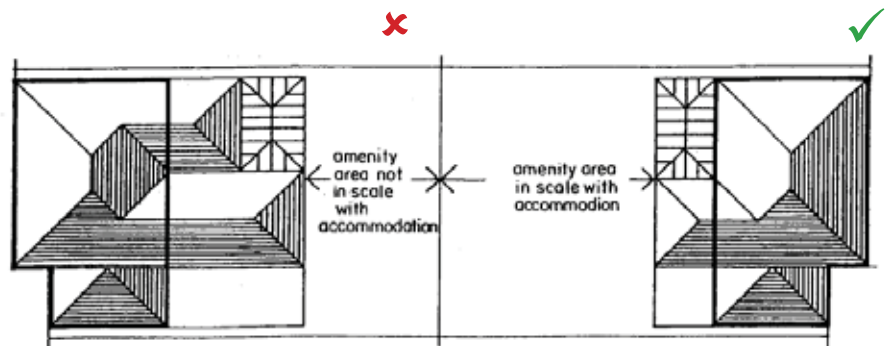
All proposals must determine whether the house is listed or located within a conservation area as in these cases there are greater restrictions on the nature of an extension. Extensions applying for planning permission will also need to justify their need for additional space and explain the merits of the proposed design with respect to its impact on the current house. All extension designs, whether in need of planning permission or not, should carefully consider the advice provided in this section.

Layout

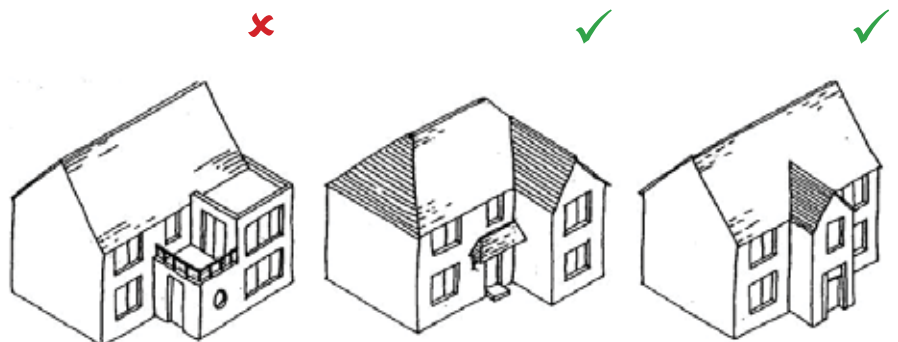
- The layout of the extension should enhance the existing property.
- The location of the extension and the position of its windows should not result in any adverse overshadowing or overbearing impact on adjacent dwellings.
- Plans and elevations should demonstrate the relationship of the proposal to the relevant part of the existing dwelling and any adjoining dwellings, especially from the street, including the position of boundaries and any significant mature trees.



Extensions must be sited to avoid harming the outlook from neighbouring dwellings (as a guide dimension 'x' should exceed 'y')



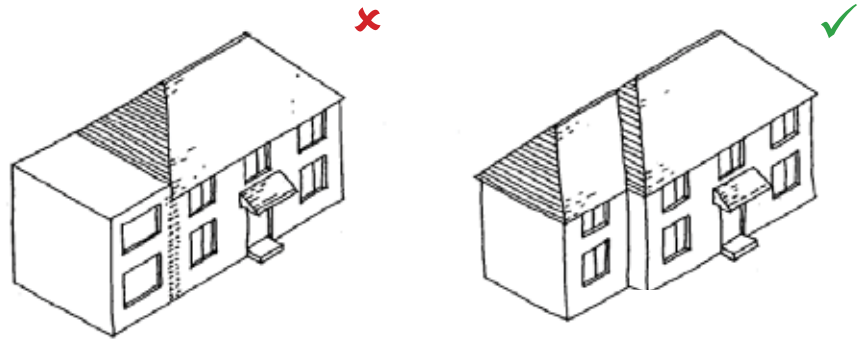
Family housing must retain reasonable levels of private garden amenity in scale with the size of dwelling.



Extensions at the front must result in a balanced architectural composition

Building form

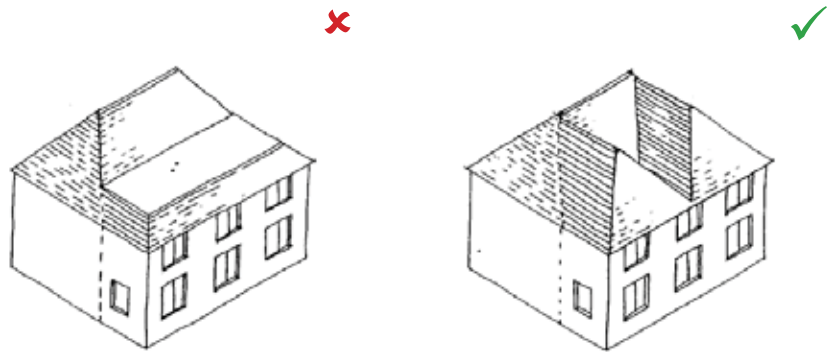
- The additional mass should respect the existing building proportion, symmetry and balance.
- The size and location of the extension should not compromise the private garden amenity of the dwelling concerned or any of its neighbours.



Roof form

- The roof of an extension is a prominent component of the building form and should normally be of a similar format to that of the existing dwelling.
- Extensions to the roof using hipped or gabled forms should have the same angle pitch as the existing dwelling.
- If height is of concern, the roof can be sub-divided into a number of smaller elements.
- Roof forms that are contrary to the existing roof form will generally be resisted.

Designing the extension as a subsidiary building element may help to provide a more balanced composition and to overcome problems of matching materials



Large roofs can be subdivided into a number of elements to reduce their bulk, in preference to using a false pitch roof

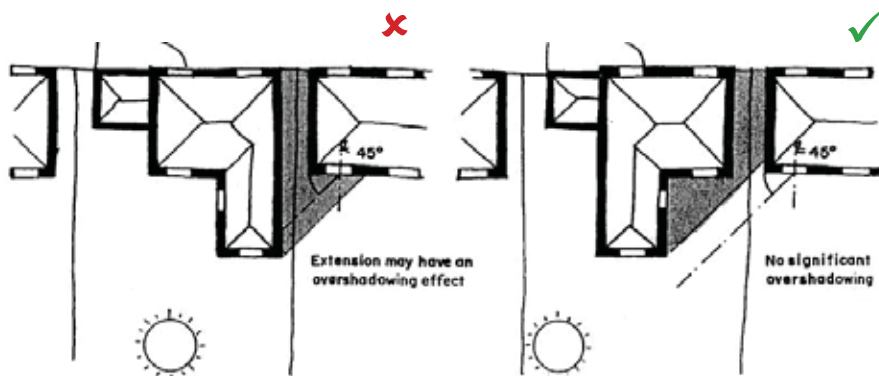
Streetscape

The architectural form of the extension is of particular importance if visible from the street.

- Boundary treatment should be well considered and in keeping with the existing building and streetscape.
- Side extensions are often the most convenient extension but can have significant impact on the character of streets. Proposals must maintain rhythm and visual separation.
- Careful consideration should be given to the impact of porches.
- Garages are usually best set back from the main building frontage especially if they are attached to the dwelling.
- Significant extensions to the street facade will usually be resisted where there is a well established building line.
- Extensions should not result in unbalanced or disproportionate frontages.



Elevation detail of front extensions should align with that of the existing building



Large two storey extensions sited close to a boundary can restrict daylight to habitable rooms of the neighbouring dwelling. Alternative siting may overcome the problem.

Amenity

- Although many trees are protected by Tree Preservation Orders, a condition of planning permission, or where the property is within a Conservation Area, it should be noted that many unprotected trees will have high amenity value and should be retained wherever possible.
- Mature shrubs and hedges, especially those close to a side boundary, can make a significant contribution towards the privacy and amenity of a private garden and should be retained where possible.
- Large two storey extensions should not be sited close to a boundary as this can restrict daylight to habitable rooms of the neighbouring dwelling.

Materials

- The materials used for an extension should match or compliment the existing building.
- New brickwork will require careful matching, including the colour of mortar and pattern of bonding.
- It is often helpful to stagger the junction between old and new to make the join less noticeable.
- Using contrasting materials for the new element can also be effective but they must compliment those used on the existing building.

Rear extensions

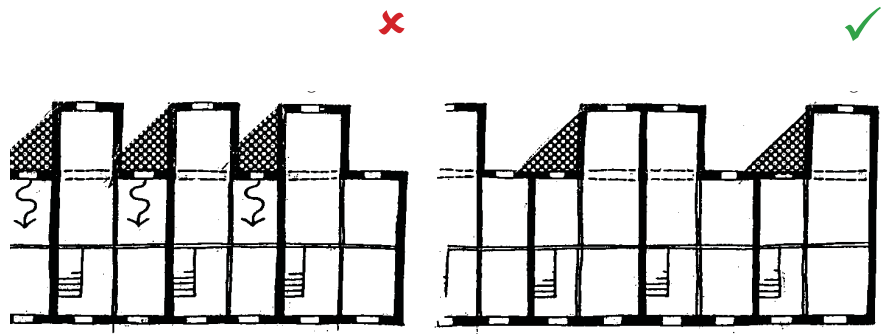
- Single storey rear extensions will usually be granted planning permission as they are unlikely to affect the public view of the building or affect the amenity of a neighbour.
- Two storey extensions, particularly if they extend beyond 3 metres from the building, need to be carefully sited as they can result in loss of daylight or have an overbearing impact on the

adjoining dwelling unless they are kept well away from the separating boundary.

- Windows above ground floor level should always face directly towards the rear garden of the property to avoid overlooking.

Side extensions

- Side extensions are often the most convenient way to extend a dwelling. However, they can also have a significant impact on the character and appearance of a property and that of the street scene.
- Two storey extensions which leave little or no space between adjoining dwellings will not be permitted if they create a 'terracing effect'.
- It is important to retain a minimum 1m gap between all two storey extensions and a side boundary. In lower density developments a much greater distance will be required.



Individual extensions cause overshadowing, whereas combined extensions result in less overshadowing.

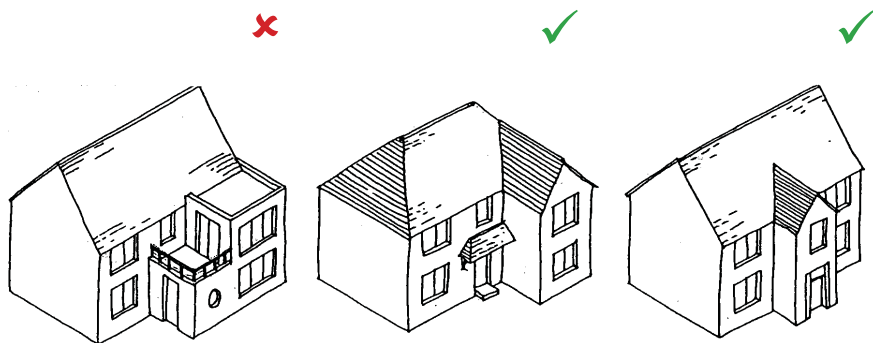


Single storey side extensions retain visual separation whereas two storey side extensions can create a 'terracing' effect.

Front extensions

The front elevation of a dwelling is of primary importance to the character and appearance of the street scene and works will usually require planning permission regardless of size.

- Significant extensions will usually be resisted where there is a well established building line or where works reduce the provision for off street parking.
- Adding a small single storey extension, such as a bay window, may be approved providing it closely matches existing architectural detail.



Extensions at the front must result in a balanced architectural composition.

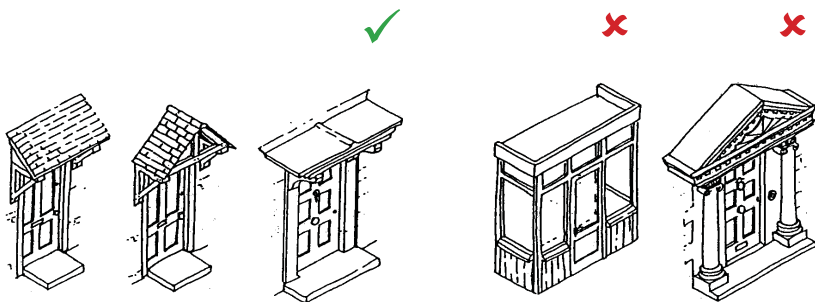
Porches

The front entrance is the focal point of a house and even small changes can radically alter its appearance, particularly in the case of a terraced house.

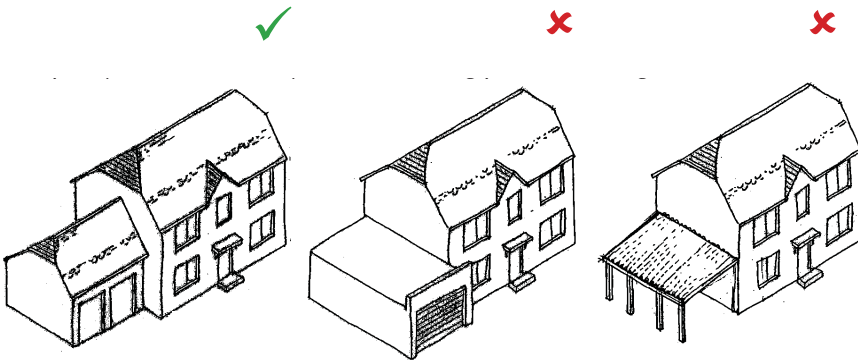
- Where appropriate simple traditionally designed rain hoods of approximately 1m in depth supported on brackets can be the



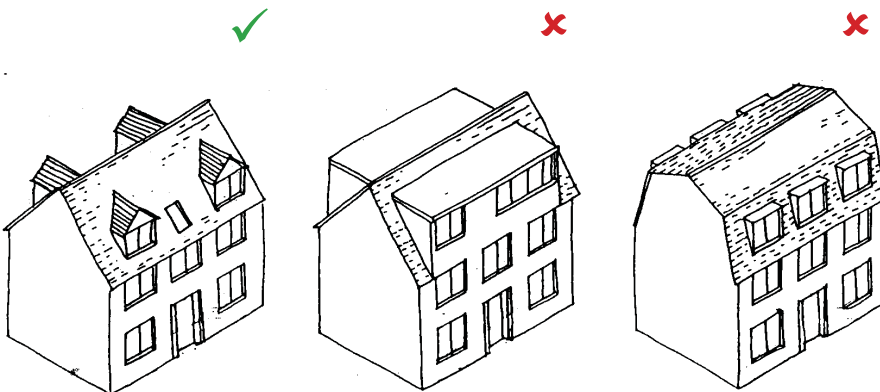
Even small extensions on the front elevation of a terrace, such as a bay window, can upset its balance and rhythm.



Traditionally designed rain hoods are less likely to appear incongruous than 'off the shelf' porches or fussy reproductions.



Garages are best sited back from the main frontage and should match the form and material finish of the dwelling.



Only modest extensions to the roof will be permitted which do not dominate its form and appearance or alter its character.

most successful.

- Fully enclosed porches rarely have an acceptable appearance unless they closely match the character of the house.

Garages

- Garages are usually best set back from the main building frontage.
- The materials used and roof form should be in keeping with those used for the main dwelling.
- Open roof structures that form car ports are unlikely to be permitted where they are in public view unless they are well designed and constructed from high quality materials.

Roof extensions and dormers

- Planning permission is not normally required for any internal conversion works or the insertion of flush fitting roof lights or small dormer windows to the rear.
- Elsewhere permission will usually be granted for the installation of traditional dormer windows if they are a subordinate feature of the roofscape, they are in keeping with the character of the dwelling and street scene and do not affect overlooking.

Good design checklist

This checklist will form part of the criteria for accessing the design of developments and it is therefore essential that all of the points are addressed.

- ✓ Does the layout of the extension enhance the existing property?
- ✓ Is the additional quantum of space appropriate to the size of the house and its grounds?
- ✓ Does the extension not compromise the garden size or amenity in any way?
- ✓ Is the relationship between old and new dealt with appropriately?
- ✓ Is the daylight to the existing house and to neighbouring properties maintained?
- ✓ Does the proposal avoid overlooking of existing neighbours?
- ✓ Does new landscaping respect existing character and positively contribute to the area?
- ✓ Has the proposal taken any opportunities to rectify previous unsatisfactory works to the property (for example eroded front boundary conditions)?

10 CLIMATE CHANGE

10.1 BACKGROUND

The National Planning Policy Framework requires new development to address the causes and potential impacts of climate change. The Council is committed to addressing issues relating to climate change through the development management process. Key relevant policies include:

- CS21 Design
- CS22 Sustainable Construction
- CS23 Renewable and low carbon energy generation

10.2 PRINCIPLES

Standards

Demonstration projects in the borough have shown that the highest standards of sustainability and environmentally sensitive design can be delivered in Woking (2+3).

The Council requires residential development on previously developed land to achieve a minimum number of credits in the energy and CO₂ and water categories of the Code for Sustainable Homes. Where justified and viable, the Council will negotiate with developers to achieve the energy and water elements in their entirety; the full requirements of the Code level or even a higher Code level. Residential extensions of 1000 sq.m or less (gross) floorspace will be encouraged to incorporate energy and water efficiency measures.

Sustainable energy and water

- Development must incorporate measures to minimise energy consumption and to conserve water resources.

1 Environmental standards requirements
 2 + 3 Dartmouth Avenue development by Greenoaks Housing Association and Jon Broome Architects - High environmental performance (EcoHomes Excellent) achieved in Woking (Ellis + Moore)

Site / development type	Minimum standard required (or any future equivalent)
Residential development on previously developed sites	From April 2013 - energy and CO ₂ and water components of Code Level 4 (encouraged to meet full requirements of Code Level 4) From April 2016 - energy and CO ₂ and water components of Code Level 5 (encouraged to meet full requirements of Code Level 5)
Residential development on greenfield sites	Code Level 5
Non residential development over 1,000 sqm	BREEAM Very Good

1



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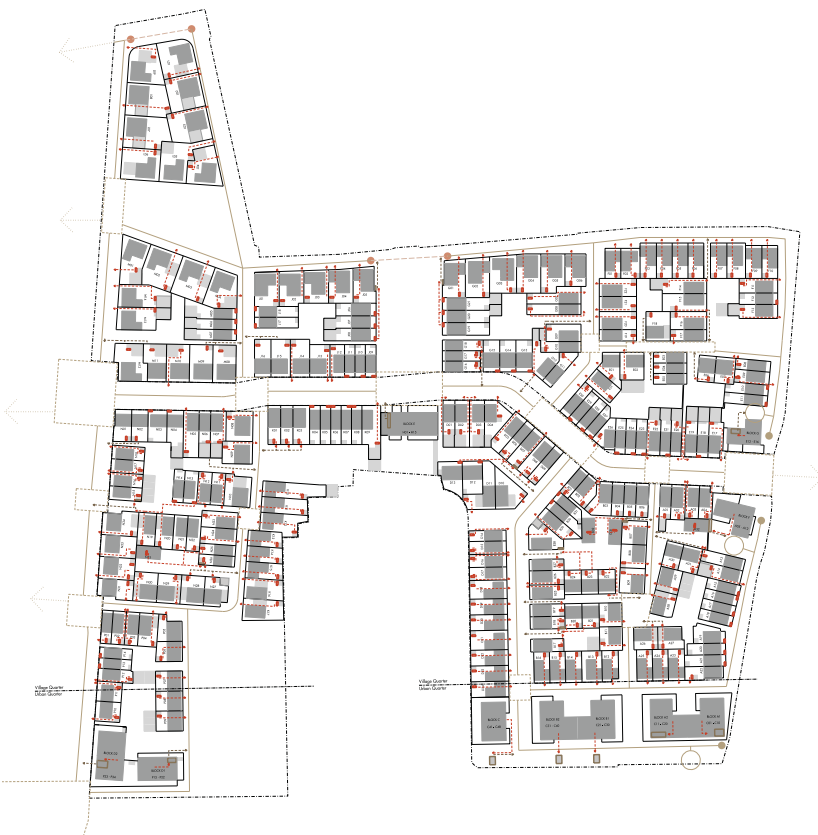
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- 4 Combined Heat and Power - BedZed
- 5 Green roofs benefit biodiversity and can provide additional amenity space
- 6 Example waste strategy
- 7 Alpine Close, Maidenhead
- 8 Attractive bicycle shed design - WWF Headquarters, Woking
- 9 Cycle parking - Commercial Way, Woking
- 10 SUDS and sustainable building materials and construction - Accordia, Cambridge

- Developers can choose which issues to address in order to achieve at least 50% of the total credits available under the energy and waste categories in the Code (or 80% from April 2016) The mandatory credits must be included.
- Developments should consider the integration of Combined Heat and Power (CHP) or other forms of low carbon district heating in the development (4).
- All new development in proximity of an existing or proposed CHP station or district heating network must be connected to it unless it can be demonstrated that a better alternative for reducing carbon emissions from the development can be achieved.
- It is the responsibility of a developer to make proper provision for surface water drainage to ground, water courses or surface water sewer.

Design, materials and waste

The following principles should be adhered to in order to maximise the benefits of passive design:

- The layout of new development should maximise the potential for passive solar gain and facilitate renewable or low and zero carbon energy installations.
- Site layout should use landscape to benefit from shelter to minimise heat losses in winter and avoid over-shadowing of the solar orientation of buildings.
- Design techniques should be applied to new buildings that maximise the capture and use of passive solar energy while avoiding excessive solar gain in summer (e.g. using brise soleil, natural ventilation, shutters, cool or reflective building materials on roofs or facades) (6).
- Site layout should provide adequate shade in summer.
- The planting of trees should be encouraged but trees must be carefully selected and positioned to avoid damage to underground infrastructure.

In addition, the design of all new developments should facilitate the reduction of waste and the recycling and composting of the waste produced. This will require appropriate space to be integrated into the design of developments and a well-considered approach to material sourcing and re-use (7).

Sustainable transport

Development must minimise users need of motorised transport through the following measures:

- Provision of travel plans.
- Provision of safe and attractive walking and cycling opportunities including, where appropriate, secure cycle parking and changing facilities.
- Ensuring the provision of car parking is consistent with cutting greenhouse gas emissions, including through providing for electric vehicle charging infrastructure.

Adapting to climate change

Dealing with the effects of climate change has implications for the design on new developments. Ensuring buildings and spaces are comfortable and attractive for their users means thinking carefully about site layout, building materials and approaches to construction, vegetation and drainage. Buildings and spaces designed to passively balance temperatures and shelter will cost less to maintain in the long term.

Green and blue infrastructure (i.e. green space, vegetation, water and drainage networks) are critical to successful adaptation to the effects of climate change. It is essential that all developments maximise the contribution this infrastructure can make to managing temperatures and water resources. Even small development interventions designing in new vegetation (such as a new street tree, a green roof or simply more generous planting) will make a positive contribution to the sustainability of a site. More detailed guidance is set out in the Woking's Climate Change SPD and the Green Infrastructure Strategy.



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11 SHOPFRONTS

11.1 CONTEXT

The quality of shopfronts has an enormous impact on the vitality and economic health of shopping streets across the borough. First impressions are formed by the environmental quality of shopping streets and good quality shopping parades can help attract further investment and help avoid vacancies and decline. The following principles act as a guide for those implementing new or renovating existing shopfronts.

11.2 PRINCIPLES

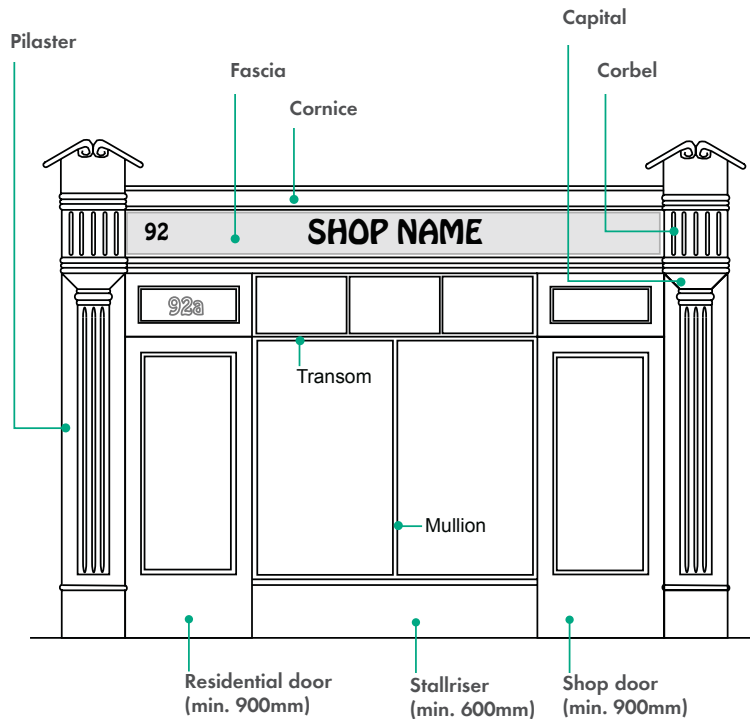
Planning Permission

Planning permission is required for the replacement of a complete shopfront, a significant alteration to an existing shopfront and the installation of a blind or canopy and shutters and grilles. The following permissions also apply:

- Listed Building Consent is required for any works which will affect the character or appearance of a Statutory Listed Building including demolition and some internal alterations.
- Conservation Area Consent is needed for the demolition of an existing shopfront in a Conservation Area.
- Building Regulations Approval may be required for works which require structural alternations or affect 'means of escape' in case of fire.
- Advertisement Consent is generally required for illuminated signs, signs at first floor level and blinds or canopies which incorporate advertisement.

Access

When altering or providing a new shop front, access requirements for disabled shoppers must be considered and can provide benefits for parents with pushchairs, people



1

1 Key architectural features of a shopfront
Courtesy of London Borough of Waltham Forest

2 Consistent fascia heights create a cohesive parade - Guildford Road, Woking

3 Proportions are well considered, design is tasteful and pilasters are maintained - Guildford Road, Woking

4 Leyton shop front improvements (Courtesy of LB Waltham Forest)

5 A consistent approach in terms of scale need not rule out diversity in shopfront design - Hereford Retail Quarter



2



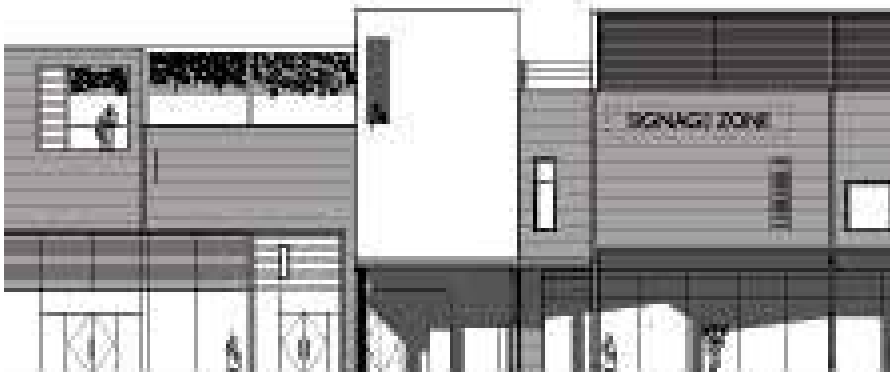
3

Existing fascia proportions and heights respected with appropriate lettering

Generous window proportions support views into and out of the shop



4



5

with shopping trolleys and the elderly and small children. Access for disabled is required under Part M of the 1985 Building Regulations, which should be referred to for technical specifications.

Alterations and new shopfronts on existing buildings

Shopfronts form an integral part of the whole building and should have a strong design relationship with the overall architectural composition.

- Alterations or new shopfronts should not obscure any existing architectural detail.
- Removing vertical glazing divisions and replacing them with large areas of plate glass on traditional shopfronts should be avoided.
- Any alterations or new shopfronts in a Conservation Area must enhance and complement the Conservation Area character.

Character

- New shopfronts should reflect the original design intent of the building or street block, being sensitive to the historic character and approach (6+7).
- The design and materials should be in keeping with the building's character and respectful of adjacent properties and streetscene (1+5).
- Original features such as pilasters often survive hidden under later work and could be incorporated into new designs (3,5+6).
- Where a traditional shopframe is used, a stallriser of at least 600mm and no lower than the height of an adjoining pilaster base should be included.

Proportion

- Shopfronts perform a vital role in the natural surveillance of a street and the design of the shopfront should reflect this role, with generous clear windows (3).
- The dominant shopfront height along a parade should be respected and the size and proportion of signage in keeping with local character (1).

- Advertising should be carefully designed with regard to the character and proportion of the shopfront, the building and adjacent shopfronts in the street scene. Similarly, materials should be chosen to empathise with the character and appearance of the shopfront and surroundings.
- The proportions of the shopfront should have a strong design relationship with upper storeys.



6

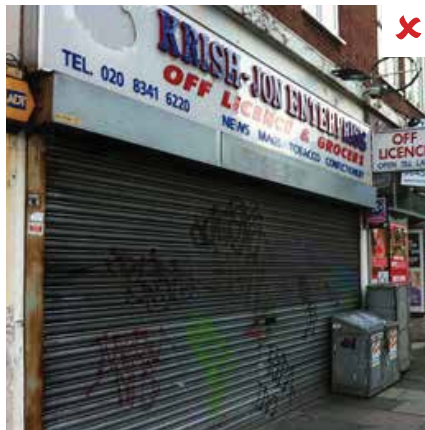


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Materials

The materials used should complement the shopfront character and appearance and, where possible, respect those of adjacent shopfronts.

- All materials should be of high quality, attractive and durable.
- All signage offers an opportunity to improve the way a development presents itself to the street and should therefore be of high quality.



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Lighting

- Lighting must be sensitively designed to minimise light pollution and help support appropriate lighting levels on adjacent walkways.
- Illumination must be carefully designed to reflect the character and appearance of the overall shopfront. Bulky, fully illuminated box signs will generally be resisted. Individually illuminated letters, fret-cutting or halo lighting can be effective if sensitively designed.
- Any proposed light fittings should avoid unsightly clutter of projecting lamps and wiring.

Security

- Security shutters should be incorporated into the design of a new shopfront from the outset. Shopfront shutters and grills, concealed behind the fascia when the shop is open should only cover the glazed parts of the retail unit and should be open mesh or grille (8+9) to maintain activity/interest at night time.

6 Shopfront design respects historic facade - Chertsey Road, Woking
 7 Shopfronts are out of scale with poor composition - High Street, Woking
 8 Solid external security shutters, such as metal roller types, which obscure shopfronts, negatively impact the street.
 9 Open-grille shutters are a visible form of security that does not compromise the external appearance of the shop



10



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14

- Traditional canvas roller or 'apron' blinds were a common feature of 19th century shopfronts and would be suitable for use on Victorian, Edwardian and most inter war shopfronts.
- The use of bright plastic blinds and other glossy materials should be avoided.
- Planning permission is normally required for blinds but those including names or logos are controlled by advertisement regulations.

Fascias and fascia signs

- New shop fascias should only extend to the edge of the individual shop unit.
- Where a shop occupies more than one building, the fascia board should be interrupted on the line of the building division.
- The height and depth of the fascia should relate to fascias on adjoining properties and should not encroach onto a string course, window sill or other features of the upper floors.
- The fascia board should generally not exceed one quarter of the height from pavement level to the bottom of the fascia panel.
- Signage should have clear lettering that complements the overall shopfront appearance.
- Multiples, which often use cheap materials and are not designed specifically for individual shops, are often inappropriate for the character of the building and may require modification if they are to be acceptable. This is of particular relevance for shopfronts in Conversation Areas.

Advertisements and signage

- Projecting signs should be positioned no higher than fascia level and restricted to one per shop.
- Traditional painted hanging signs on metal brackets should be used on older period properties.
- Internally illuminated signs should be avoided.
- Signs positioned above the ground floor fascia level will be resisted.

10 Traditional character enhances streetscene - Whitechapel High Street

11 Hanging signs should be located within or below the fascia level - Commercial Way, Woking

12 Internally illuminated signs will be resisted - Heath Road, Twickenham

13 Maintaining traditional features such as pilasters helps to articulate individual units and maintain historic character - Whitechapel High Street

14 Individually illuminated letters - Jones Bootmaker shopfront

PART E - GOOD DESIGN & THE PLANNING PROCESS

12 PLANNING APPLICATION REQUIREMENTS

12.1 INTRODUCTION

This final section of the report provides information to assist those preparing planning applications for sites in Woking. In tandem with other relevant guidance, it provides advice which will be helpful in encouraging applicants to present their proposals in context. It is hoped that, through this process of wider context analysis, the overall quality of schemes in the Borough can be improved.

This approach is not intended to promote a slavish copying of existing forms. Rather, it is intended to promote a considered response to the wider area, promoting good contextual design in a wide variety of interpretations. Well considered and carefully composed contemporary approaches to design, even in the most sensitive locations, are encouraged but it is important to ensure development proposals are presented by the appropriate level of supporting information.

Woking Borough Council offers a pre-application advice service and it is highly recommended that you use this before submitting an application.

12.2 CONSIDERATIONS RELATING TO KEY THEMES

Large projects

Large projects naturally have a higher requirement in terms of the volume of information required for the submission. However, a great deal of this information is technical in nature and there is still a need for the applicant to demonstrate that the design of any proposals are informed by and respond positively to the prevailing character of the area and the wider borough. Masterplans and design codes should be considered for larger schemes.

Urban form

The most fundamental aspect of the planning and design of major new development is its overall urban form and the connections it makes with surrounding streets. Schemes should be presented with sufficient context to demonstrate how they integrate with the wider form of the area, creating linkages where possible and maximising opportunities for integration. Good practice examples of block form and structure from around the Borough may also be used as comparators to demonstrate that the form being proposed is relevant and appropriate to the context.

Development typology

The Council will be looking to see that the type of development proposed is relevant to the context. This will be reflected in the scale of buildings and plots and the overall form of development. This process is not intended to limit the scope for new forms of development. However, it will be helpful in demonstrating that the proposals are an intelligent response to context. This can be illustrated through drawn and photographic study of examples from the immediate area and good practice examples from the wider borough in the Design and Access Statement.

Smaller projects

For small projects such as extensions, new individual buildings or small groups of buildings, the expectation is that the level of information required to judge a planning application will be proportionate and reasonable. However, it will still be expected to provide sufficient information to enable the Council to make an informed judgement about the scheme. A planning application, and any pre-application discussion which precedes it will greatly benefit from information which demonstrates the way in which the design of the proposals relates and responds positively to the surrounding character, the local urban form and the wider character of the borough.

When considering the prevailing character of a street, it is important to show more than the buildings immediately adjoining the plot. Information is needed to demonstrate the level of consistency in the scale and character of the area to help gauge whether the proposals constitute an informed response to context.

It is also recommended that this work to demonstrate the context is prepared as an early stage in the preparation of any scheme design. This will help to inform the design process, leading to a better quality of scheme. The analysis should also make reference to relevant sections of the Woking Character Study and the Heritage of Woking document, highlighting the character type and using the character description and information from the study to inform and support their design work.

12.3 SUPPORTING INFORMATION

More detailed guidance relating to the information required to be submitted with planning applications of different types is outlined on the Woking Borough Council website. Applicants may also wish to refer to Woking's *Design and Access Statements: Guidance for Applicants*. The following elements of work are considered important in presenting the design for a planning application in a proper context:

Plan

The proposals should be shown in relation to the surrounding context on all sides. A plan with sufficient context is helpful in demonstrating that the scheme relates appropriately to surrounding buildings with particular regard to the following:

- Plot rhythm – a repeated plot width along the length of a street establishes a rhythm which is an important feature of the character of the area. New development which significantly alters this rhythm will dilute the character of the street. Plans submitted will be expected to demonstrate how they reinforce or respond to the existing plot form;
- Building line – the building line along a street is a key characteristic of the place. In areas where there is a high degree of consistency, applications should demonstrate that the building line will be adhered to; and
- Land use and parking – the plan is important in allowing for an assessment of uses. A key example is the impact on adjoining owners where sites are redeveloped and new parking areas are incorporated which have an impact on adjoining gardens.

Street elevations

The front elevation of the proposed development should be shown in sufficient context to allow it to be assessed alongside the neighbouring buildings.

It will be helpful for the graphic style of the proposals and the existing adjoining buildings to be the same to allow for an accurate comparison.

The elevations should allow for the height of the proposed building to be assessed against its neighbours, along with the shape and proportion of elevations, arrangement of windows and other key features.

Planting in front gardens is a key feature of many streets in the Borough. In some examples it may be beneficial to prepare a version of the street elevation in which the planting is shown as well as the main building elevation drawing. This will help to demonstrate the impact that planting can be expected to have in maintaining the character of the street. However, the expression of trees and planting in drawn views should not simply be used to obscure the appearance of new development in supporting material.

The street elevations should ideally take the form of drawn images. However, for smaller schemes it may be appropriate for the application to be supported by a photographic elevation to provide a clear understanding of the street.

Massing

A key issue for many developments is the way in which they intensify the use of existing plots. Whilst this is an appropriate response in many cases, sufficient information needs to be presented to demonstrate that this approach is not detrimental to the character of the area. Whilst elevations and plans are important in presenting any scheme, sketches or views which explain the proposals in three dimensions can also be extremely helpful. These can be hand drawings, images from simple computer models or photography of simple physical models. The level of time and effort given to preparing this information will depend on the scale of the proposed development.



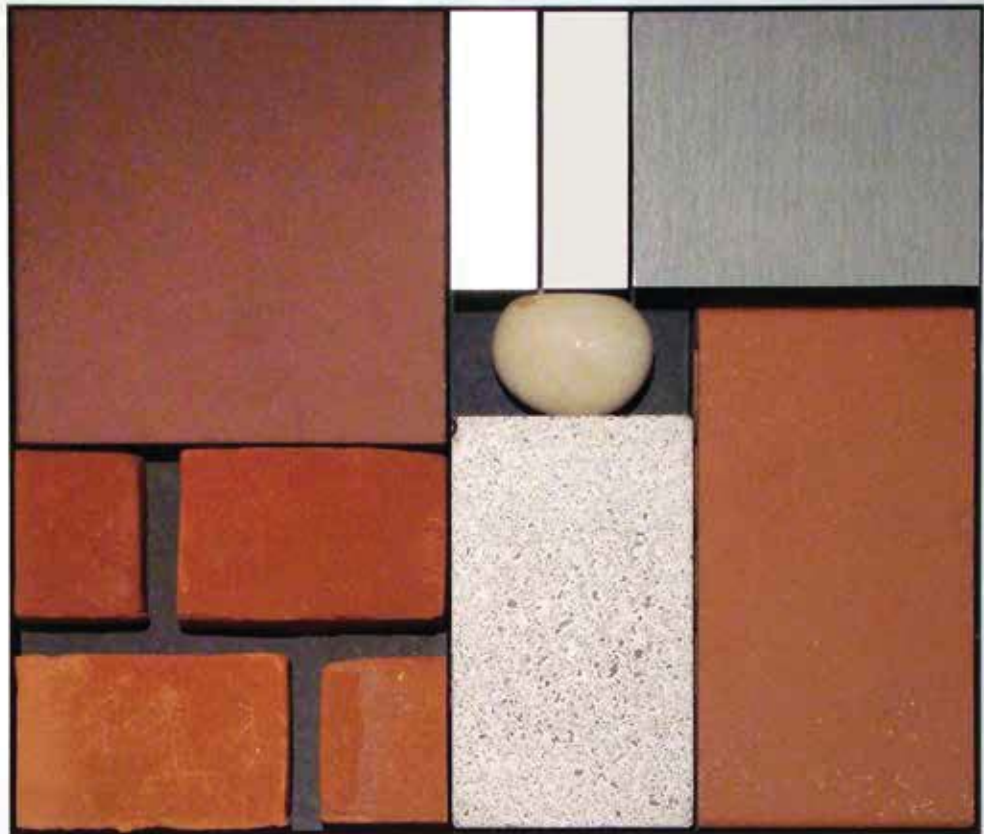
ROOFS

- 1 Clay tile
- 2 Prepatinated zinc
- 3 Pebble ballast

WALLS

- 4 Brick
- 5 Precast concrete
- 6 Terracotta
- 7 Anodised aluminium
- 8 Polyester powder-coated metal

1	7 8	2
	3	
4	5	6



LANDSCAPE

- 9 Granite sets
- 10 Reinforced gravel
- 11 Precast concrete
- 12 Brick pavers

BICYCLE SHED

- 13 Cedar cladding

9	10	11
13		12



Example of a materials palette

Materials and details

Materials and details proposed for a scheme should be supported by evidence which demonstrates that they are suitable for the context. The quality of detailing on built developments, of both the buildings and the landscape, has a major bearing on ability of schemes to make a positive contribution to existing urban environments. The use of local materials can contribute to local distinctiveness. Details should be provided which demonstrate issues relating, although not necessarily limited to:

- Window design;
- Rainwater goods;
- Brick and other material choices;
- Pointing;
- Eaves detailing;
- Soldier courses and quoins;
- Roof design and detailing;
- Boundary treatment;
- and landscaping.

It may be beneficial or necessary to support particular choices with photographs of clearly identified local examples. Materials and detailing are likely to be controlled by the use of planning conditions.



The Lightbox
Gallery
Museum
Shop
Café
Free Entry

THE BUILDING
FOR LEARNERS
AND VISITORS
TO VISIT
Free Entry

The Lightbox
Gallery
Museum
Shop
Café
Free Entry

The Lightbox
Gallery
Museum
Shop
Café
Free Entry

LIBRARY

St Andrew's
House

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APPENDICES



The Lightbox
Gallery
Museum
Shop
Cafe
Free Entry

THE LIGHTBOX
GALLERY
MUSEUM
SHOP
CAFE
FREE ENTRY

THE LIGHTBOX
GALLERY
MUSEUM
SHOP
CAFE
FREE ENTRY

THE LIGHTBOX
GALLERY
MUSEUM
SHOP
CAFE
FREE ENTRY

St Andrew's
House

LIBRARY

A1 WOKING TOWN CENTRE ANALYSIS

A1.1 INTRODUCTION

This section presents a series of analysis plans of Woking town centre which have been prepared in order to inform a tall building strategy for Woking. In doing so, the information presented provides a useful overview of the range of assets, issues and opportunities relating to the centre. It will therefore be important that development proposals coming forward take proper account of the analysis provided here and seek to respond positively to the issues and opportunities identified.

For Woking town centre, information on the following is provided:

- 4.2 Historical evolution of the town centre;
- 4.3 Heritage assets;
- 4.4 Topography;
- 4.5 Public realm and open space;
- 4.6 Building heights;
- 4.7 Land use character;
- 4.8 Town centre structure;
- 4.9 Positive townscape features;
- 4.10 Negative townscape features; and
- 4.11 Potential development sites.

A1.2 HISTORICAL EVOLUTION OF THE TOWN CENTRE

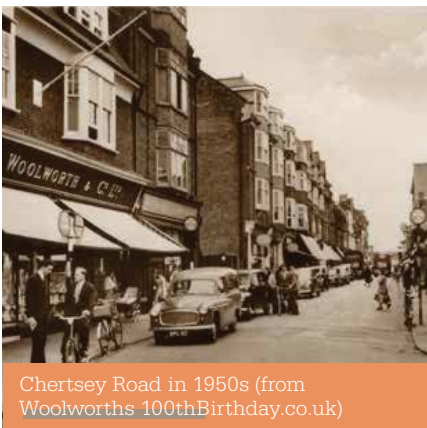


Inward looking street frontage

Woking town centre grew and intensified progressively around a formal structure between the mid 19th century and the mid 20th century. However, in the 1960s and 1970s the town centre was substantially redeveloped, losing much of its historic form. The town centre expanded in the 1980s and became dominated by large scale buildings with little frontage on to the external streets.



Blended early C20 map laid over existing map of Woking



Chertsey Road in 1950s (from Woolworths 100th Birthday.co.uk)

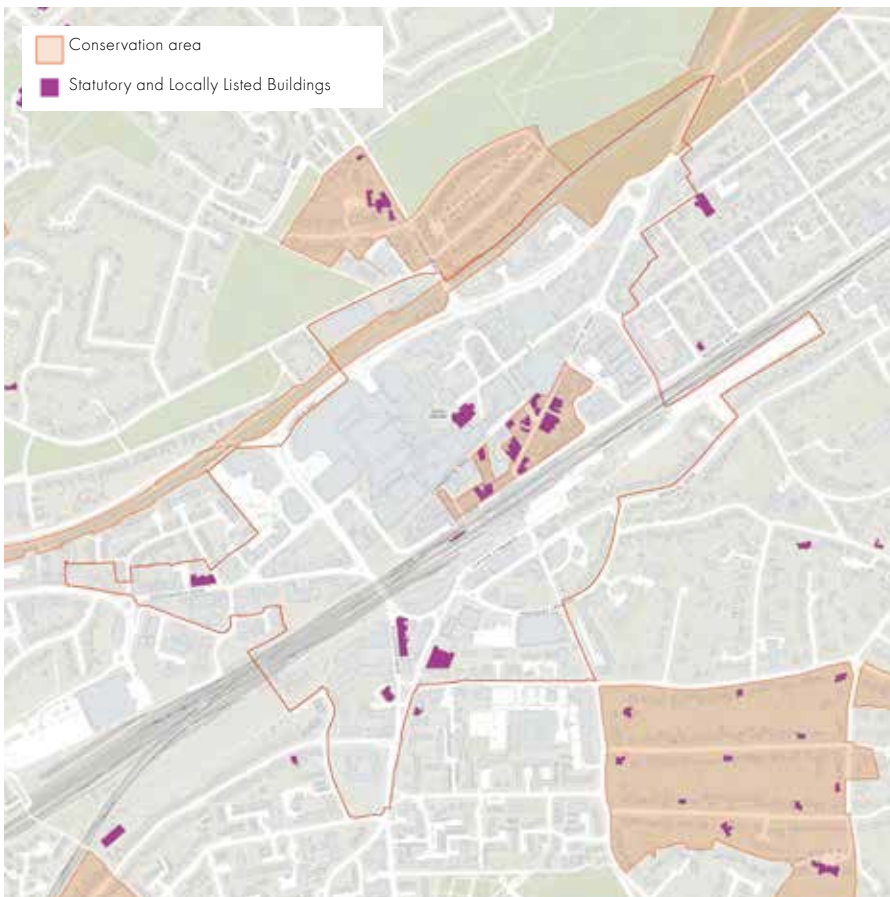


Woking town centre's original formal layout in the early 19th century (Woking library)

Key issues/implications

- Despite the loss of historic built form many of the historic formal street alignments are still evident.
- Much of the town centre urban form has become inward looking with little positive street frontage.
- The settings of surviving historic buildings need enhancement.

A1.3 HERITAGE ASSETS



Designated heritage assets in Woking



Historic and high quality street frontage



The Central Conservation Area has the only remaining historical urban grain



Christ Church is a local landmark and a Grade II listed building

There are relatively few heritage assets within the town centre boundary due to the substantial redevelopment of the core. A cluster of historic buildings and streets exists on the north side of the railway station, creating an attractive gateway entrance into the shopping area. The canal and its environs are designated Conservation Areas and are valuable assets to the town.

Key issues/implications

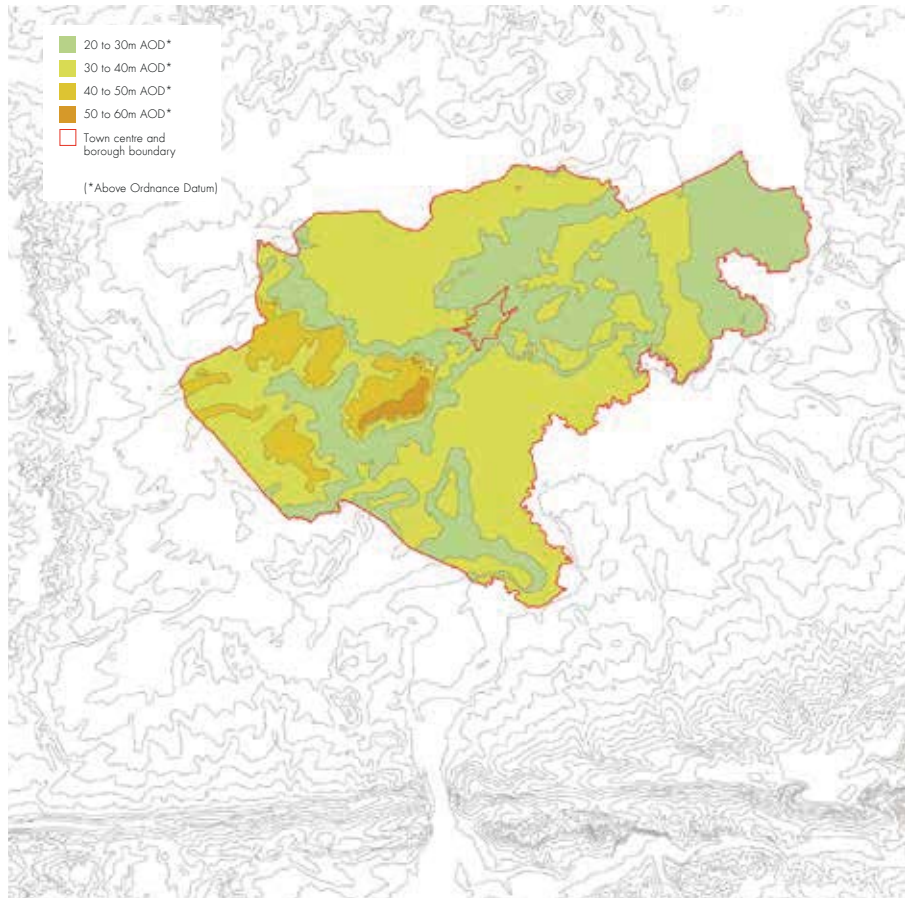
- The Central Conservation Area, its listed buildings and its setting need protection and enhancement.
- The canal is an important but hidden asset.

A1.4 TOPOGRAPHY



The borough is generally flat but with some attractive gentle hills to the south and west

The town centre sits on flat land, with the only changes in topography coming from land rising towards the station. There is a gentle descent into the centre from the south. Across the wider borough there are few views from higher land, although the view looking north from the Hog's Back is a strategic view.



Borough-wide topography



On the south side of the rail line, streets rise gently up to the station entrance on the elevated tracks

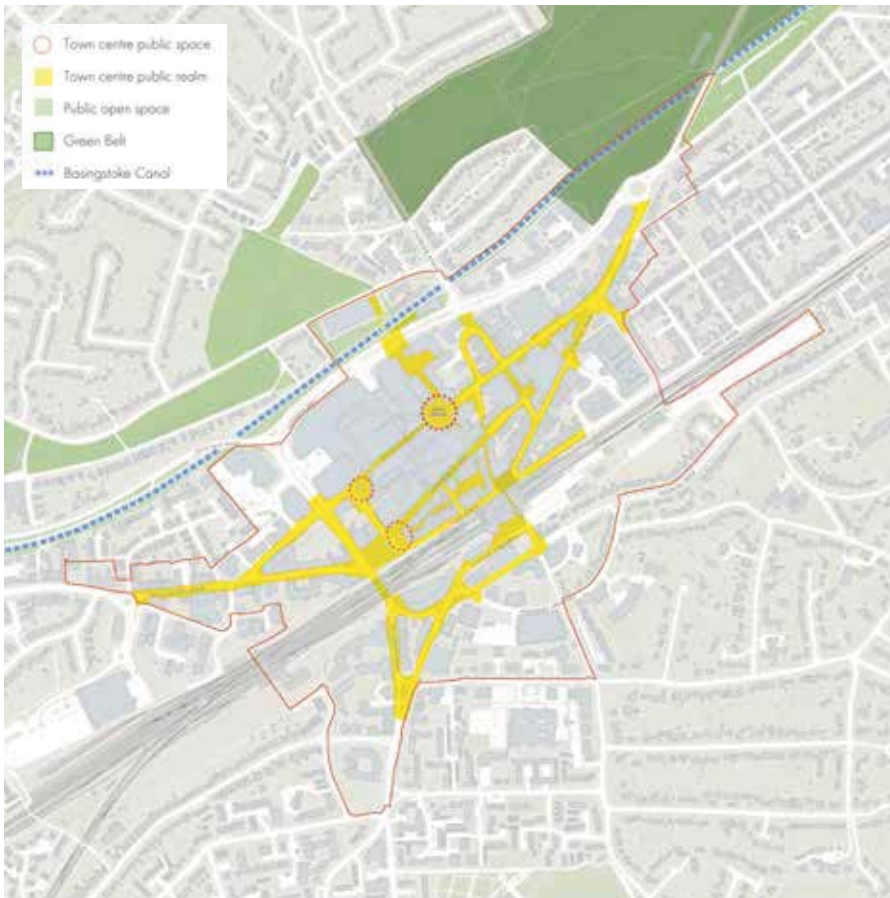


The Basingstoke Canal runs through the centre of Woking

Key issues/implications

- Attractive, gentle descent into the centre from south.
- No particularly prominent or important views from higher land.
- Town centre on relatively flat land.

A1.5 PUBLIC REALM AND OPEN SPACE



Public realm and open space in Woking



The Basingstoke Canal



Jubilee Square



Victoria Way

There is very limited green space within the town centre although there are some key hard landscaped spaces in prominent locations, including recent improvements of Jubilee Square. Around the edge of the town centre, Victoria Way presents an unforgiving environment for pedestrians. The most significant green space resource is to the north of the centre, including the canal, tow path and Wheatsheaf Common.

Key issues/implications

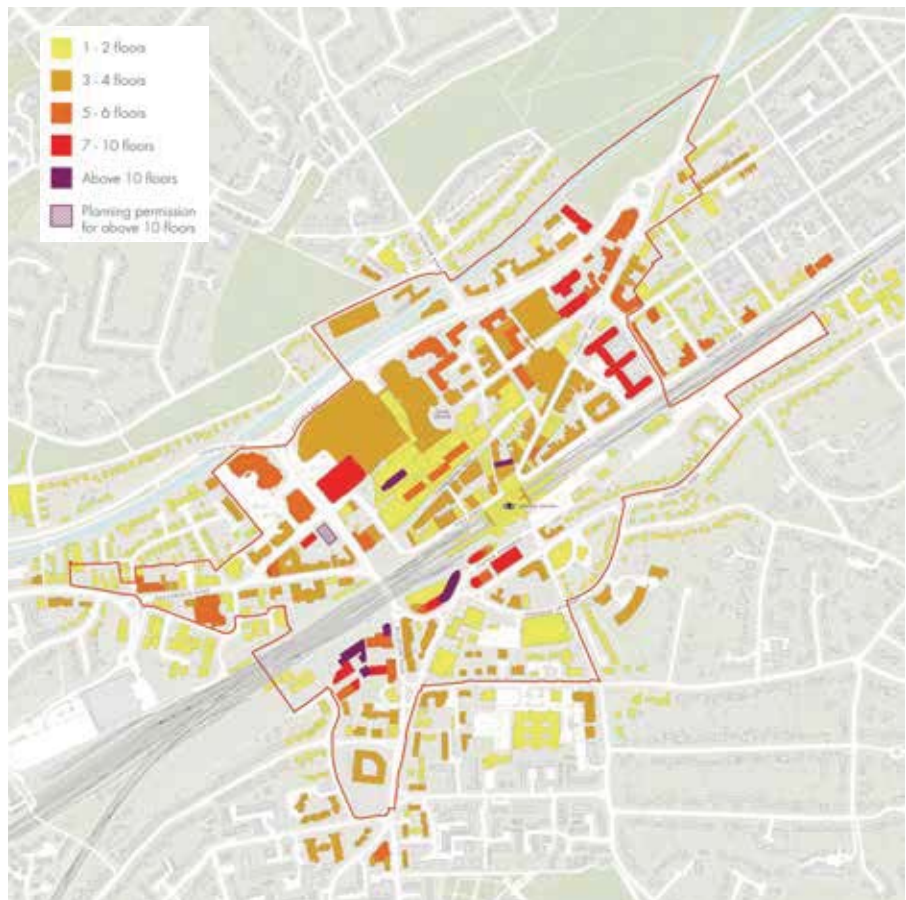
- Very little green space or soft public realm in the town centre.
- Extensive green spaces and assets to the north but with poor links to the town centre.

A1.6 BUILDING HEIGHTS



Location of tall buildings in centre lack clear structure

Buildings in the town centre are markedly taller than those elsewhere in the borough. Within the town centre buildings are generally 5-10 storeys on principal central routes, with the tallest buildings tending to be closer to the railway corridor. As development pressure continues to increase in the centre more guidance on tall buildings is required. Beyond the town centre buildings are typically 2-3 storeys.



Building heights in Woking



The fringe areas of the town centre are dominated by 2-3 storey buildings, as on Guildford Road

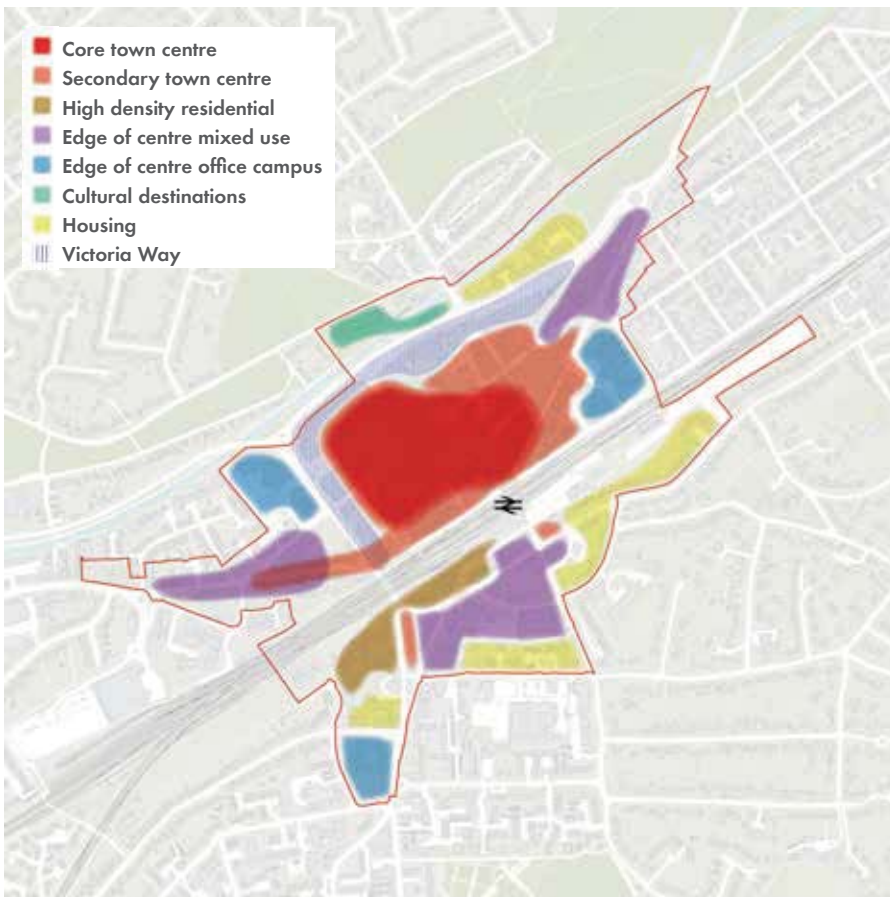


Over the past decade the town centre has seen a number of taller developments

Key issues/implications

- The location and distribution of taller buildings in the centre does not present a clear, legible structure.
- A strategy to direct future tall buildings is required.
- A sensitive approach is needed to the historic built form within the centre where a 3-4 storey pattern prevails.

A1.7 LAND USE CHARACTER



Land use character in Woking



Large offices along Victoria Way



Recent high density residential development has significantly increased the number of people living in the town centre



A mix of uses exist at the edge of the town centre, with some potential for redevelopment to enhance the character of these gateways

Woking town centre has a large retail core. Some residential uses exist above retail units in a number of locations. Office and commercial uses are located along routes running out from the edge of primary retail activities. High density housing has been introduced in the south-west quadrant of the centre over the last decade, with pressure for more of this type of development. Victoria Way runs around the north side of the town centre and has its own character with a mix of large office and cultural uses.

Key issues/implications

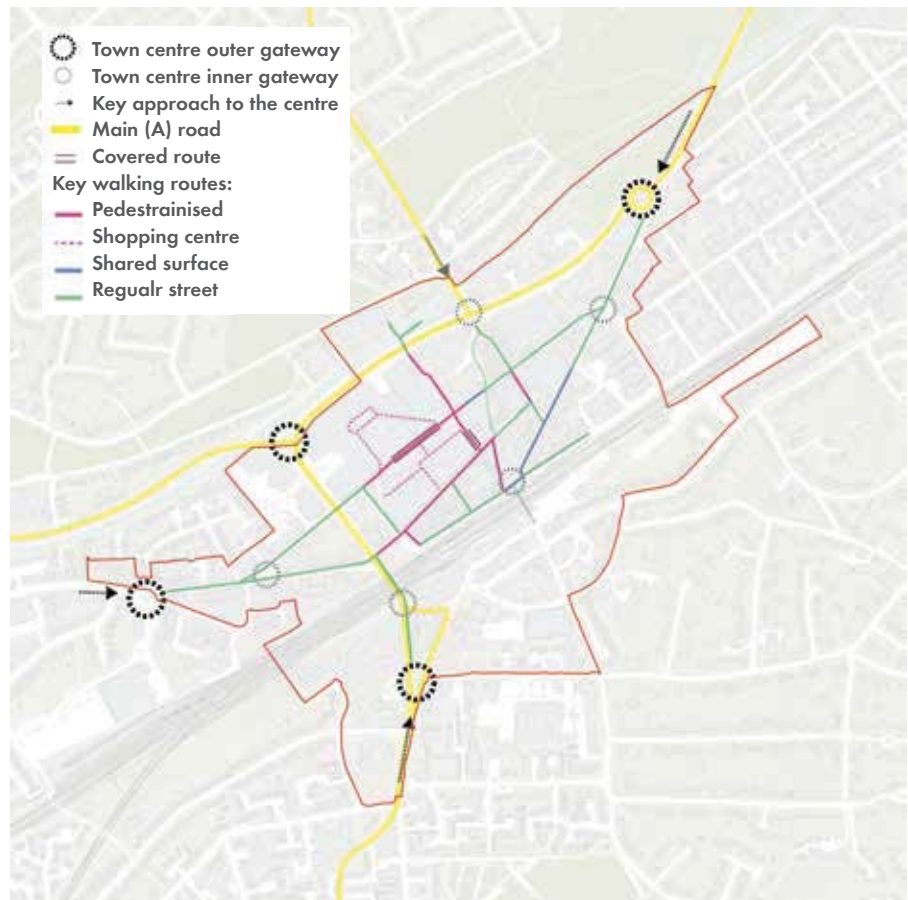
- Much of the retail core is inward-looking.
- There are opportunities to enhance the edge of centre mixed-use areas.
- Uses along Victoria Way could be reworked to support better connections between the town centre and immediate hinterland.

A1.8 TOWN CENTRE STRUCTURE



Strong gateway from South along Guildford Road culminates in key pinch point under rail bridge

Woking's core is dominated by pedestrian streets, whilst Victoria Way feeds traffic in and around the edge of the centre. The transition between vehicular and pedestrian streets is sometimes awkward and has resulted in street clutter and informal vehicle parking and waiting. The series of primary (vehicular) and secondary (pedestrian) gateways across the town centre provide the basic structure for town centre operations – now and in the future.



Town centre structure in Woking



Main traffic route - Victoria Way

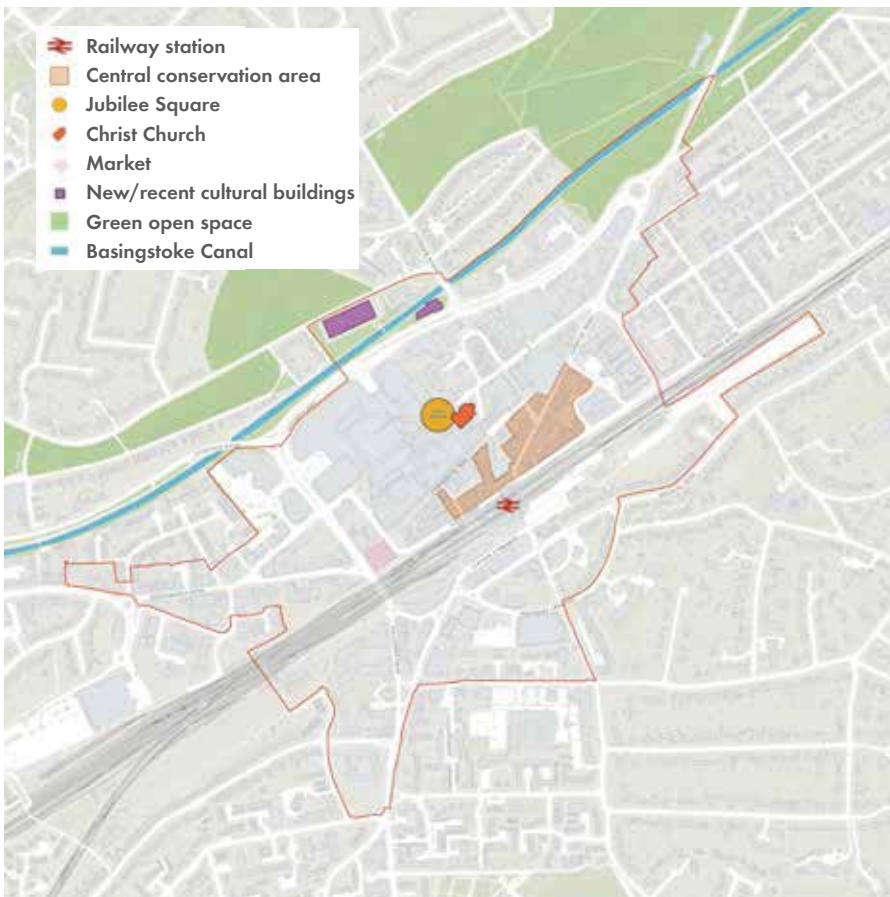


A key east-west axis needs to operate better for pedestrians and cyclists - Church Street East

Key issues/implications

- Historic street routes provide underlying structure and should be respected.
- Visibility of town centre is limited due to vehicular bypass and pedestrianised central streets.
- Relatively clear gateways exist but many do not give a high quality impression of the town centre.

A1.9 POSITIVE TOWNSCAPE FEATURES



Positive townscape features in Woking



Christ Church and the new Jubilee Square



Canal Corridor



Historic Built Fabric - Chertsey Road

The Central conservation area is one of the key assets of the town centre. Christ Church and the new Jubilee Square are particularly attractive elements. The open space and canal corridor to the north of the town centre are strong landscape assets but both are currently under used and undervalued. However, the Lightbox and the WWF headquarters are bringing new activity to this area.

Key issues/implications

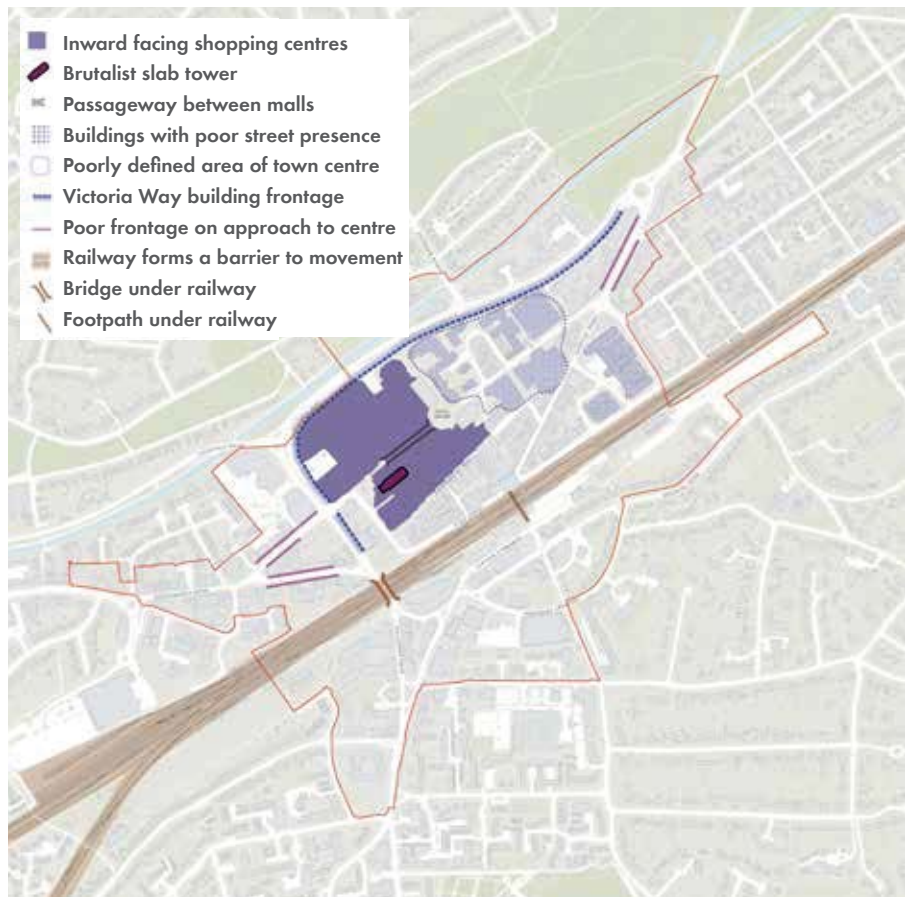
- The remaining historic built fabric needs to be well protected as this is one of the few higher quality townscapes.
- The canal corridor provides a highly attractive route and environment that at present is little used. Better connections could bring this asset greater recognition.

A1.10 NEGATIVE TOWNSCAPE FEATURES

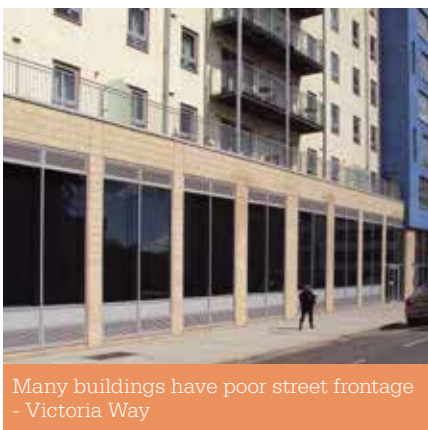


Limited opportunities to cross the railway

Large sections of the town centre are dominated by inward looking retail developments or buildings with poor street frontage. Victoria Way has little positive frontage and acts as a barrier for pedestrian and cyclists. Parts of some streets are not well defined due to poorly considered redevelopment over the past 30-40 years which has removed the historic, positive relationship between buildings and streets. These areas tend to coincide with gateways into the town centre.



Negative townscape features in Woking



Many buildings have poor street frontage - Victoria Way



Inward looking retail - Commercial Way

Key issues/implications

- Limited positive examples of building to street relationships.
- Links under the railway are limited and congested, restricting flow north to south.
- Definition and character could be significantly improved through redevelopment based on perimeter block formats.

A1.11 POTENTIAL DEVELOPMENT SITES



Albion House, Woking



Recent public realm improvements in the town centre



Centrium Development on Station Approach



Barratt scheme on Guildford Road

There is significant development pressure in Woking Town Centre with a number of buildings proposed, under construction or recently completed. These include the Victoria Square development, the cluster of buildings around Victoria Way south, the WWF Headquarters, the Magistrates Court and the recently completed Barrett scheme just south of the railway. There is potential for further development along approach routes to the centre and within the town centre itself.

Key issues/implications

- Intensifying pressure for development, with a concentration of new buildings emerging around the railway.
- Recent public realm improvements have positively impacted the town centre.
- Potential for further development along approach routes.



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A2 WEST BYFLEET ANALYSIS

A2.1 INTRODUCTION

This section presents a series of analysis plans of West Byfleet - the Borough's second largest retail centre. In doing so, the information provides a useful overview of the range of assets, issues and opportunities relating to the centre. It will therefore be important that development proposals coming forward in West Byfleet take proper account of this analysis and seek to respond positively to the issues and opportunities identified.

The following information is provided in relation to West Byfleet:

- A2.2 Historical evolution;
- A2.3 Heritage assets;
- A2.4 Building heights; and
- A2.5 An overview of West Byfleet opportunities.

A2.2 WEST BYFLEET HISTORICAL EVOLUTION

West Byfleet developed as a commuter suburb following the opening of the station in 1887. The area is primarily characterised by inter-war development, although the historic core has some older buildings. The town is adjacent to the M25, which separates the village from Byfleet. The Canal runs between West Byfleet and the residential areas of Woodham and New Haw to the north.

Historically, there were two separate shopping areas, one immediately adjacent to the station and the other along Old Woking Road. Post-war redevelopment effectively joined these two areas creating a significantly larger retail and service area.



1960



Sheer House

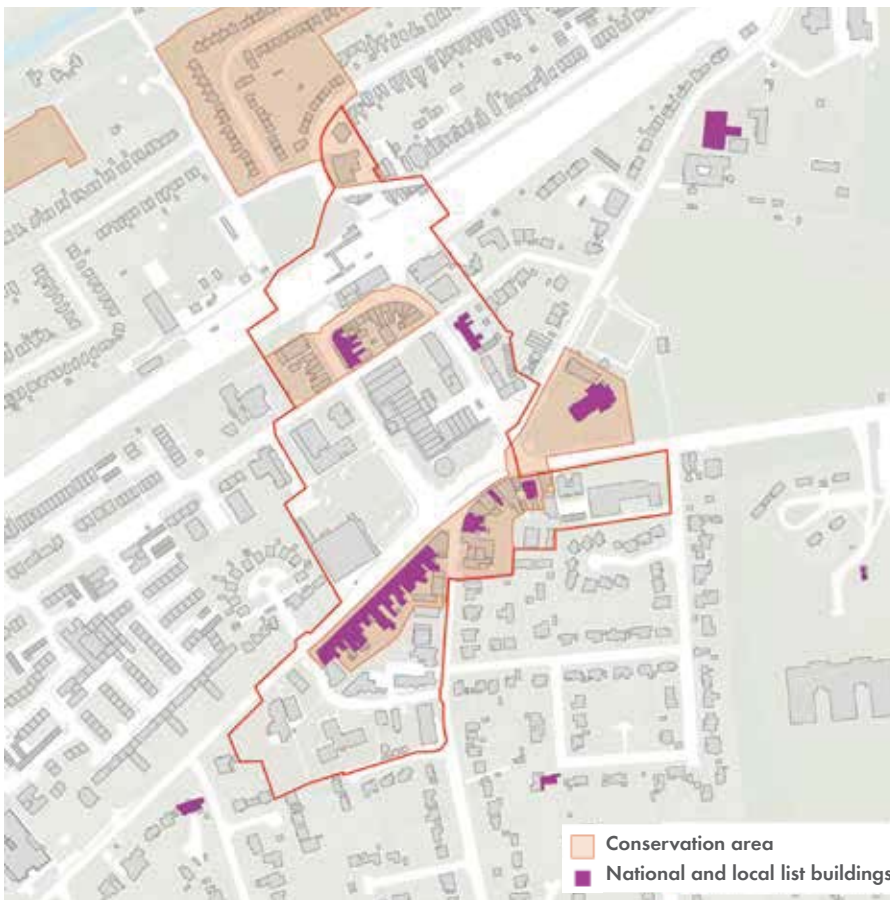


Historic character

Key issues/implications

- Redevelopment has not followed the traditional character and structure.
- Rail line acts as a barrier to the north since the town's development.
- Streetscape quality has declined as buildings have increasingly been set back from the street edge.
- Car dominated.

A2.3 WEST BYFLEET HERITAGE ASSETS



Old Woking Road



Station Approach



St John The Baptist Church

Conservation areas cover the two historic parts of the centre around the station and Old Woking Road. A number of listed and locally listed buildings exist, primarily located in the conservation areas.

Key issues/implications

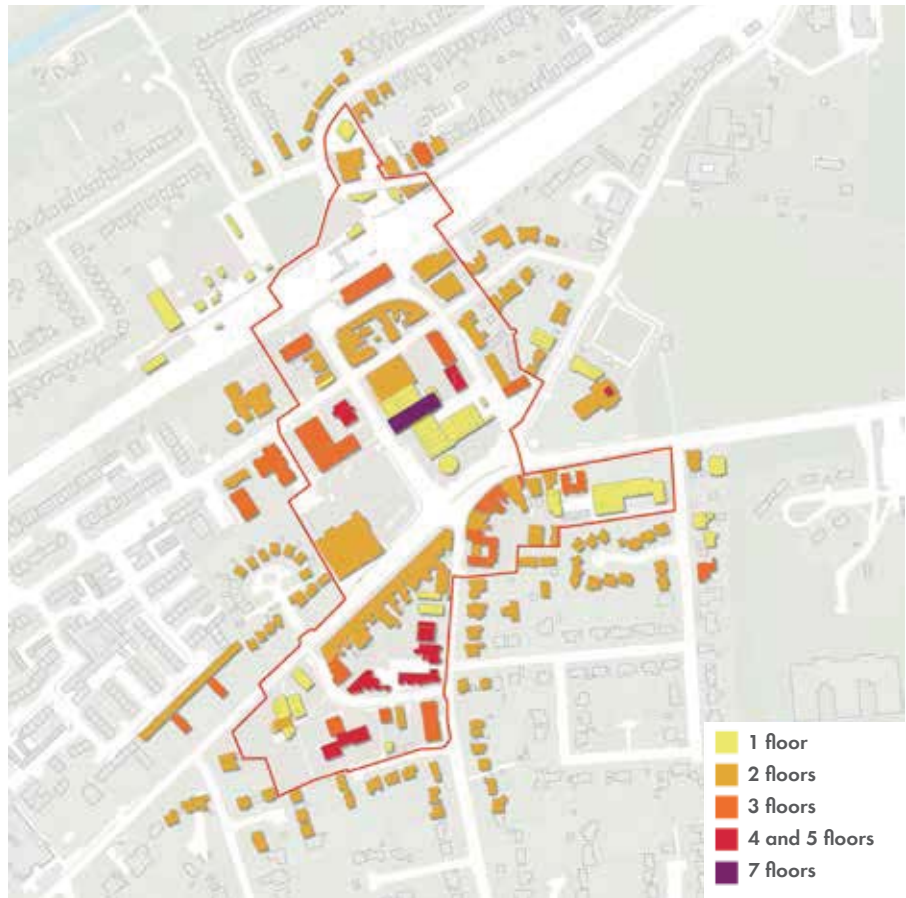
- St. John the Baptist's Church remains an important historic landmark.
- Historic frontage opposite the station and along Old Woking Road give West Byfleet an attractive character and image.
- Station should be improved to match the quality of the surrounding streetscape.

A2.4 WEST BYFLEET BUILT CHARACTER - HEIGHT



West Byfleet has a generally low-rise character with mostly two storey buildings.

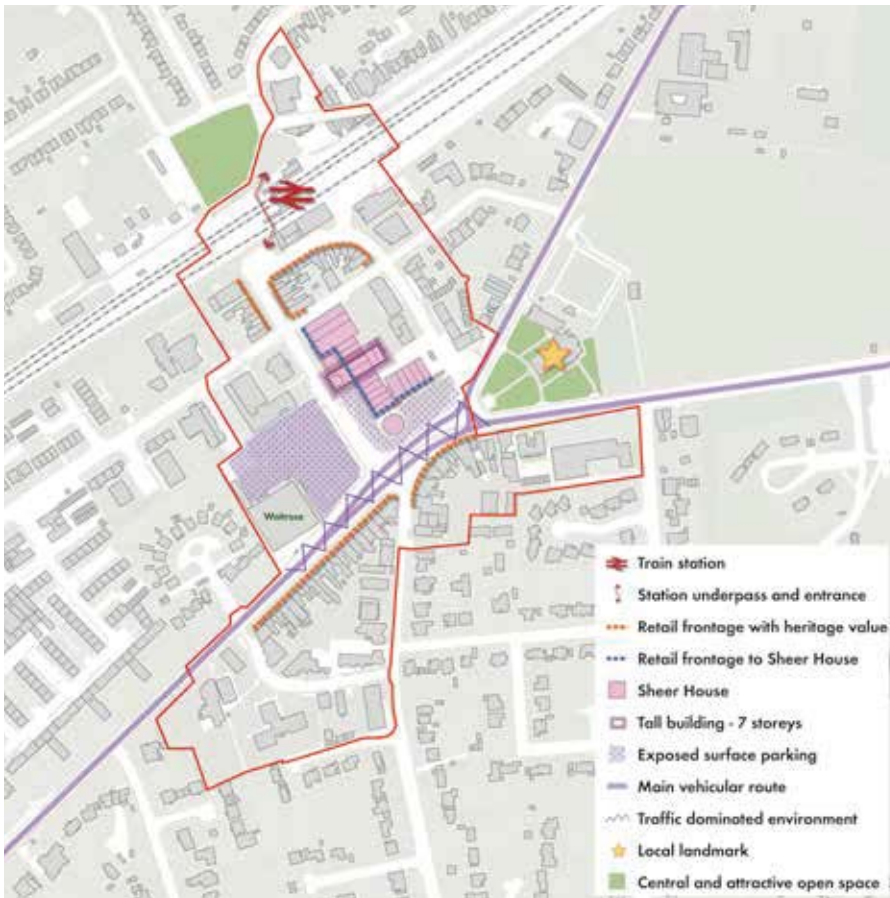
Some modern housing developments have pushed densities higher at 3-5 storeys. The Sheer House slab is a prominent and incongruous exception at 7 storeys.



Key issues/implications

- West Byfleet has a finer grain and lower scale than Woking town centre.
- The scale of the Sheer House development is at odds with prevailing scale and character.

A2.5 WEST BYFLEET OPPORTUNITIES



The station



West Byfleet Common



Car dominated public realm

The train station needs enhancement to match the quality of the surrounding area. The Sheer House development is outdated in form and its comprehensive redevelopment would present a major opportunity for West Byfleet. The current green spaces in the centre are poorly connected and there is a general lack of pedestrian public space.

Key issues/implications

- The train station building and entrance needs improving.
- Redevelopment of the Sheer House site would significantly enhance the town centre.
- There needs to be better connection between green spaces.
- The centre needs more pedestrian public space.

A3 ADDITIONAL DESIGN RELATED DOCUMENTS

The following documents, or the future equivalents, provide useful design related guidance and should be referred to in addition to the Woking Design SPD as required:

- Residential Boundary Treatments SPG (2000)
- Telecommunication Masts and Antennae SPG (2000)
- Heritage of Woking (2000)
- Parking Standards SPD (2006)
- Outlook, Amenity, Privacy & Daylight SPD (2008)
- Woking Character Study (2010)
- Climate Change SPD (2013)
- Hot Food Takeaway SPD (2014)
- Affordable Housing Delivery SPD (2014)
- Good Practice Guide on Light Pollution (draft)
- Conservation Area Appraisals

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